

REPORT AMENDED BY CPC 9-24-87
CITY PLANNING COMMISSION

1231 "I" STREET, SUITE 200, SACRAMENTO, CA 95814

APPLICANT	Julie Arnold, 7728 Wilbur Way, Sacramento, CA 95828		
OWNER	Carl Panattoni, 7728 Wilbur Way, Sacramento, CA 95828		
PLANS BY	Leo McGlade Associates		
FILING DATE	6-30-87	ENVIR. DET.	Cat. Ex. 1506(b)(3) REPORT BY DJH:tc
ASSESSOR'S-PCL. NO.	237-022-044		

APPLICATION: Plan Review to allow construction of two warehouse/office buildings totaling 103,680+ square feet on 5.03+ vacant acres in the Light-Industrial-Review (M-1(S)-R)

LOCATION: West side Kelton Way 300+ feet south of Main Avenue

PROPOSAL: The applicant is requesting the necessary entitlements to construct two warehouse buildings of 45,360+ square feet and 58,320+ square feet each.

PROJECT INFORMATION:

1974 General Plan Designation: Industrial
1984 North Sacramento Community Plan Designation: Industrial
Existing Zoning of Site: M-1(S)R
Existing Land Use of Site: Vacant

Surrounding Land Use and Zoning:	Setbacks:	Required	Provided
North: Warehouse/Office & Vacant; M-1(S)R	Front:	25 ft.	25 ft.
South: Single Family; R-1	Side(Int):	5 ft.	13 ft.
East: Single Family; R-1	Side(St):	0 ft.	0 ft.
West: Furniture Mnft; M-1(S)R	Rear:	0 ft.	5 ft.

Parking Required: 135 spaces @ 20% Office and 80% warehouse
Parking Provided: 135 spaces
Property Dimensions: 633' x 364'
Property Area: 5.03+ acres
Square Footage of Building: Office = 20,736 sq. ft.;
Warehouse = 82,944 sq. ft.;
Total = 103,680 sq. ft.
Height of Building: 29 ft., one-story
Topography: Flat
Street Improvements: Existing
Utilities: Existing
Exterior Building Materials: Concrete tilt-up
Roof Material: Tar
Estimated Number of Employees: 153

Project History:

In 1971 the site was approved for a rezone from Agriculture to Light Industrial Park (M-1S-R) zone uses were allowed.

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APPLC. NO. P87-292 MEETING DATE: ~~August 13, 1987~~ 9-24-87 ITEM NO. ~~9~~ 5

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accenting trees is recommended with at least 50 percent of the trees along Kelton Way as evergreen trees. All landscaped areas are to have automatic irrigation.

2. Wall Requirement: The Zoning Ordinance requires a minimum six foot high solid masonry wall separating residential from non-residential uses. Staff field inspected the site and noted that Kanowsky Furniture, locate due west of the site has an eight foot high solid wall. The wall is located at the original base grade elevation with the parking lot finish grade elevation nearly four feet higher so that the effective wall height is only four feet. Staff recommends that the new six foot high wall be constructed at the top of the fill slope or on top of a retaining wall. The six foot height of the wall shall be measured from the warehouse side grade elevation. The wall of the building will also provide a buffer for any potential noise from the north side of the building.
3. Reorient Building A: Building A has two proposed below grade loading docks which will generate noise to the residences to the south. Staff recommends that building A be reoriented so that the loading bays face to the north. The building will be required to observe the storm drainage easement. Landscaping is recommended between the two buildings. In turning the building the rear wall will serve as a noise barrier.
4. Landscape - North Property Line: Staff reviewed the landscaping and recommends a 10 foot wide landscape strip along the north property line to be planted with trees, shrubbery and living ground cover. A six foot high decorative wrought iron fence is recommended to provide security and an aesthetically pleasing transition to the residential area along Kelton Way and up to the Havlin Witkin setback. Chain link fencing is appropriate for the remaining north fence line. Parking along the north property line shall include a two foot vehicle overhang into the landscape strip. No portable concrete wheel stops will be allowed. All landscape areas are to be bordered by a six inch raised poured in place concrete curb. Razor ribbon wire shall not be used. Due to the truck loading dock locations opposite a row of parking along the north property line, the normal 105 foot distance required for a semitruck-trailer combination movement would be reduced to 99 feet. Staff recommends the total landscape width along the north property line opposite Building B's loading docks be six feet in width and that the spaces be reserved for compact cars only. This would maintain the minimum 105 feet for vehicle maneuvering with the repositioning of Building B to the north by 12 feet. Refer to Exhibit A for the location of the reduced landscape planter.
5. Parking: A total of 135 parking spaces are shown on the proposed site plan based upon a maximum of 20 percent office and 80 percent warehouse use.

Office Area = 20,736 square feet @ one space per 400 = 52 spaces
 Warehouse = 82,944 square feet @ one space per 1,000 square feet = 83 spaces
 Total = 1,355 spaces

City Ordinance allows up to 25 percent office use in conjunction with a warehouse use by right. Office use over 25 percent requires a Planning Commission Special Permit. Any change in square footage or use mix will require modification to the Plan Review. All surface parking areas and maneuvering areas are required to meet the minimum 50 percent shading requirement.

6. Lighting: No detailed lighting was provided for staff review. Staff recommends that all lighting be directed on-site and not reflect off-site. Use of low level pole lights and boxed fixtures shall be included for building permit review. Lamp poles shall not exceed 20 feet in height. Any lighting proposed along the south property line or west property line shall not glare onto residentially zoned property to the south.
7. Exterior Loudspeakers: Problems with exterior public address systems have developed along Pell Drive for residents of Bollenbacher Avenue. Staff recommends that no external public address system be allowed for any tenant. Paging systems within each building are permissible so long as not audible from outside the building.
8. Parking Lot Fingers: In revising the site plan, staff recommends that no more than 15 consecutive parking spaces be shown without installation of tree well finger planters. The fingers will break up the long monotonous appearance of parking spaces without variation.
9. Trash Enclosure: The revised site plan shall include trash enclosures meeting trash enclosure guidelines. Trash enclosures are to be located more than 100 feet from residential uses.
10. Wingwall - Building B: Staff recommends that a 10 feet high solid masonry wing wall (decorative material) be constructed adjacent to the east side of the east loading dock on Building B for the 60 feet of length of the dock. The purpose of the wall is to deflect sounds from loading dock activity. Staff also recommends attaching a rolling gate constructed of decorative wrought iron to the wall and continue the fence to the north property line.

C. Elevation Evaluation:

Staff has major objections to the proposed project's elevations. Due to the proximity to single family dwellings, the stark contrast between two, 50,000 square foot warehouse buildings and residences is drastic. Staff proposes

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the following changes in order to lessen the impact such large massive buildings have on adjacent residential uses. Overall, staff would prefer the total redesign of the project to smaller commercial buildings similar to those being built along Pell Drive in the City and North Market and Northgate Boulevard in the County.

1. Building B - East Elevation

The 180 feet of Building B facing Kelton Way is stark, bland and uninteresting. Staff recommend the applicant redesign the east elevation to have more three dimensional relief by adding store front windows and either project or recess them with use of awning or canopy feature so that the wall has movement and interest. Windows shall be reviewed and approved by the design review coordinator.

Staff also notes that no relief is proposed on the concrete panels. Staff recommends the applicant incorporate a two to three foot high formliner along the top elevation of each panel with two, three inch wide, one inch deep reveal lines. The formliner and reveal bands are to circle the buildings. In addition, all walls facing the south property line shall include vertical bands or relief features at each joint. The vertical feature may be painted a trim color compatible with the overall project.

2. Colors:

No colors were indicated by the applicant. Staff recommends that elevations visible from Kelton Way be a sandblasted exposed aggregate surface similar to the buildings constructed at 110 and 120 Main Avenue. Paint is allowed on the window trim and accent trim. The roll-up doors and entry doors shall be painted a color to match the building. the color scheme for both buildings shall be reviewed and approved by the Planning Director. Colors shall be earth tone and be compatible with the colors present on the single family dwellings east across Kelton Way and to the south along Bollenbacher. Staff recommends a sandblasted concrete finish on all exterior surfaces rather than painted walls.

D. Tenants:

No specific tenants are identified for use of the buildings. The Zoning Ordinance allows a variety of uses in the M-1 zone which may create potential conflicts with the residences in the area. Staff recommends that the applicant record in lease arrangements that no activity is to occur where noise, dust or emissions are generated which violate noise or air quality standards. No outdoor storage of materials is to occur which is not screened.

Staff also recommends that the hours of operation of the facility be restricted to 6 a.m. to 9 p.m. Monday through Friday and 8 a.m. to 6 p.m. on

Saturday and Sunday.

E. Off-Site Impacts:

1. Kelton Way Parking:

Tenants may use Kelton Way for parking. In order to minimize commercial impacts on the residential area, staff recommends that the applicant require future tenants to not park trucks or automobiles on Kelton Way. The street shall be posted "No Overnight Parking" adjacent to the project site. All trucks are to be parked on-site.

2. Kelton Way and Main Avenue Intersection:

Staff has received a center expressing concerns regarding the level of improvements at the Kelton - Main Avenue intersection. They feel the project will create an adverse traffic situation if 153 employees and increased truck traffic is added to the intersection. Area residents are concerned that the City has not improved the intersection to be safer. The City Traffic Engineer commented that the intersection is not operating at an unsafe level and that the design is adequate.

F. Signage:

The applicant has not submitted any signage for the project. Staff recommends that applicant submit a detailed sign program for the entire site. Staff recommends one monument identification sign with no internal illumination. The monument sign shall e a maximum height of six feet. Any storefront signs facing Kelton Way shall not be illuminated. All other signs shall comply with the City Sign Ordinance.

G. Agency Review:

The proposed project was reviewed by the City Engineer, Traffic Engineer, Building Inspection and Meadows Development Association with comments received as noted. The Meadows Development Association has contacted staff and has submitted a letter (Exhibit B) and petition prior to the Commission hearing.

The City Engineers office commented that necessary on-site drainage would be required and that a minimum of 10 feet shall be shown for the driveway off Kelton from the property line to the edge of the driveway.

ENVIRONMENTAL DETERMINATION: The proposed project is exempt from environmental review pursuant to STate EIR Guidelines (CEQA, Section 15061(b)(3)).

STAFF RECOMMENDATION: Staff recommends that the Commission take the following actions:

Approve the Plan Review subject to conditions and based upon findings of

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fact which follow:

Conditions: Plan Review

1. The applicant shall prepare a revised site plan, landscaping and irrigation plan for review and approval by the Planning Director prior to issuance of building permits (Refer to Exhibit A).
2. The applicant shall revise the exterior elevations to the satisfaction of the Planning Director and Design Review Coordinator. All colors and material are to be included in the revised elevations.
3. Future tenants shall be informed in their lease arrangements that no activity is to occur where noise, dust or emissions are generated.
4. Every use, activity, and process shall be so operated that regularly recurring noises are not disturbing or unreasonably loud, and do not cause injury, detriment, or nuisance to any person. Every use, activity and process in business and industrial areas shall be so operated that regularly recurring noises, as detected by the human sense of hearing, without instruments, at adjoining residential district boundary lines, shall not exceed the normal level generated by uses permitted in warehouse and office districts. All uses are to comply with the City Noise Ordinance.
5. Every use shall conform to the rules and regulations of the Air Pollution Control District, County of Sacramento, and these regulations, and shall serve as minimum guidelines for determining permissible emissions. No outdoor storage of materials is to occur which is not screened.
6. Hours of operation shall be restricted to be from 6 a.m. to 9 p.m. Monday through Friday and 8 a.m. to 6 p.m. on Saturday and Sunday. No delivery or work is to occur during the evening hours.
7. All tenants shall be informed that no parking is allowed on Kelton Way for either cars or trucks. All vehicles are to be parked on the premises. Conditions 3 and 5 are to be recorded in all lease agreements. The Planning staff shall review and approve the wording in the lease agreement prior to issuance of building permits.
8. The applicant shall submit a uniform sign program for the review and approval by the Planning Director prior to issuance of any sign permits.
9. Kelton Way street frontage is to be posted "No Overnight Parking".
10. Lighting design shall be such as not to produce a hazardous and annoying glare to motorists, building occupants, or to the general public.

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11. Lighting is to be oriented away from the properties adjacent to the site.
12. Minimum parking dimensions shall correspond to the City Zoning Ordinance, except that the front two feet of all stalls, the areas into which the vehicle bumper overhangs, shall be incorporated into adjacent landscape or walkway improvements. No individual prefabricated wheel stop will be permitted. A continuous six inch raised poured in-place concrete curb shall be provided along all landscape areas abutting parking or drives.
13. A minimum 50 foot street setback from building shall be provided on Kelton Way if building height remains 29 feet.
14. Building height shall be limited to 29 feet.
15. Building setback from residential areas shall be 25 feet.
16. The Kelton Way setback may be decreased to within 25 feet if the height is reduced to between 10 to 15 feet from the proposed 29 feet.
17. Temporary structures will only be permitted if they are attendant to the construction of a permanent building and shall be placed at the start of construction and removed at completion of construction.
18. Such structures shall be as inconspicuous as possible.
19. Large items such as air conditioning, ventilating, or other mechanical equipment shall be screened or enclosed in such a manner as to hide such equipment. The design and material of the screening shall be compatible with the building architecture.
20. The projections shall be painted to match the roof or building.
21. All mechanical equipment, utility meters and storage tanks shall be screened and architecturally designed to be an integral part of the building.
22. Penthouses and mechanical equipment screening shall be of a design and material similar to and compatible with those used in the buildings.
23. Underground utility services throughout the project will be required.
24. All mechanical equipment shall be located in such a manner so as not to cause a nuisance or discomfort from noise, fumes, odors, etc.
25. Finish building materials shall be applied to all sided of a building which are visible to the general public and the occupants of other buildings.

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- 26. Untextured concrete block exposed to the exterior or large surfaces of untextured tilt-up concrete panels shall not be acceptable unless approved by the City of Sacramento Planning Department.
- 27. The general overall atmosphere of color shall be natural tones. Wood, natural stone, brick and dark anodized aluminum finishes, etc., shall be the background colors. Accent colors shall be used whenever necessary.
- 28. GARAGE, LOADING DOCK, AND OTHER SERVICES AREAS

These elements shall be so located as to cause no nuisance to the general public or occupants of the other buildings.

- a. The walls of the trash enclosure structure shall be constructed of solid masonry material with decorative exterior surface finish compatible to the main structure. Split face concrete block finish is recommended. The decorative finish is only required on walls facing the public street.
- b. The trash enclosure structure shall have decorative heavy gauge metal gates and be designed with cane bolts on the doors to secure the gates when in the open position.
- c. The trash enclosure facility shall be designed to allow walk-in access without having to open the main enclosure gates.
- d. The walls shall be minimum six feet in height, more if necessary for adequate screening.
- e. The perimeter of the trash enclosure structure shall be planted with landscaping, including a combination of shrubs and/or climbing evergreen vines.
- f. A concrete apron shall be constructed either in front of the trash enclosure facility or at point of dumpster pickup by the waste removal truck. The location, size and orientation of the concrete apron shall depend on the design capacity of the trash enclosure facility (number of trash dumpsters provided) and the direction of the waste removal truck at point of dumpster pickup.

The minimum dimensions of the concrete apron for a single, two cubic yard dumpster shall be: width 10 feet or width of enclosure facility; length 20 feet. Larger trash enclosure facilities shall require a larger concrete apron, subject to the approval of the City Building Inspections Building Technicians (Plan Checker).

Paving material shall consist of 5 inches aggregate base rock and six inches portland cement paving.

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- g. The trash enclosure shall be located a minimum of 100 feet from any residential zone or use. No unenclosed trash bins are allowed. No outdoor storage of refuse is allowed.
 - h. The enclosure shall be adequate in capacity, number and distribution.
29. The following Sign Regulations shall be reflected in preparation of a uniform signage program for the site.

SIGN REGULATIONS:

- a. A sign program shall be submitted to the City Planning staff if submitted subsequent to the City Planning Commission special permit hearing.
- b. Flashing, moving, or audible signs will not be permitted.
- c. In no case shall the wording of signs describe the products sold, prices, or any type of advertising except as part of the occupant's trade name or insignia.
- d. No signs shall be permitted on canopy roofs or building roofs.
- e. No sign or any portion thereof shall project above the building or top of the wall upon which it is mounted.
- f. All attached building signs shall be placed flat against the building.
- g. Freestanding pole signs shall not be permitted with the exception of one monument sign.
- h. No sign shall be specifically designed or oriented to be viewed from Main Avenue.
- i. No signs perpendicular to the face of the building shall be permitted.
- j. No signs shall be attached to the Kelton Way frontage which are illuminated.

DESIGN REQUIREMENTS, SIGNS

- a. Signs shall be located only as shown on the approved schematic plan.
- b. All electrical signs shall bear the UL label and their installation must comply with all local building and electrical codes.

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- c. No exposed conduit, tubing, or raceways will be permitted.
- d. No exposed neon lighting shall be used on signs, symbols, or decorative elements.
- e. All conductors, transformers, and other equipment shall be concealed.
- f. All signs, fastenings, bolts, and clips shall be of hot dipped galvanized iron, stainless steel, aluminum, brass or bronze.
- g. All exterior letters or signs exposed to the weather shall be mounted at least three fourth inch (3/4") from the building to permit property drainage.
- h. The location of all openings for conduit and sleeves in sign panels of building wall shall be indicated by the sign contractor on drawings submitted to the Design Review Coordinator. The installation shall be in accordance with the approved drawings.
- i. No signmakers' labels or other identification will be permitted on the exposed surface of signs, except those required by local ordinance which shall be located in an inconspicuous location.
- j. Painted lettering or signs shall not be permitted.

DESIGNATED PARK PROJECT IDENTIFICATION SIGN

- a. One non-illuminated monument sign as defined by Section 3.520 of the City Sign Ordinance shall be allowed for the Industrial Park.
- b. Maximum area of sign: 48 square feet.
- c. Maximum height of sign: 12 feet from street or parking lot grade, whichever is less.
- d. Location: to be located at the major entry off Kelton Way. The sign may be placed in the setback area; however, it must be located farther than 10 feet from the public right-of-way and from any driveway. No sign shall be allowed in the public right-of-way.

INDIVIDUAL BUSINESS/BUILDING SIGNS

One attached identification sign per building.

- a. Attached Sign: vertical height of sign or letters, including logo, shall not exceed two feet and overall size of sign shall not exceed 60 square feet. Each sign and business name shall consist of individual raised letter type. No canned plastic signs are

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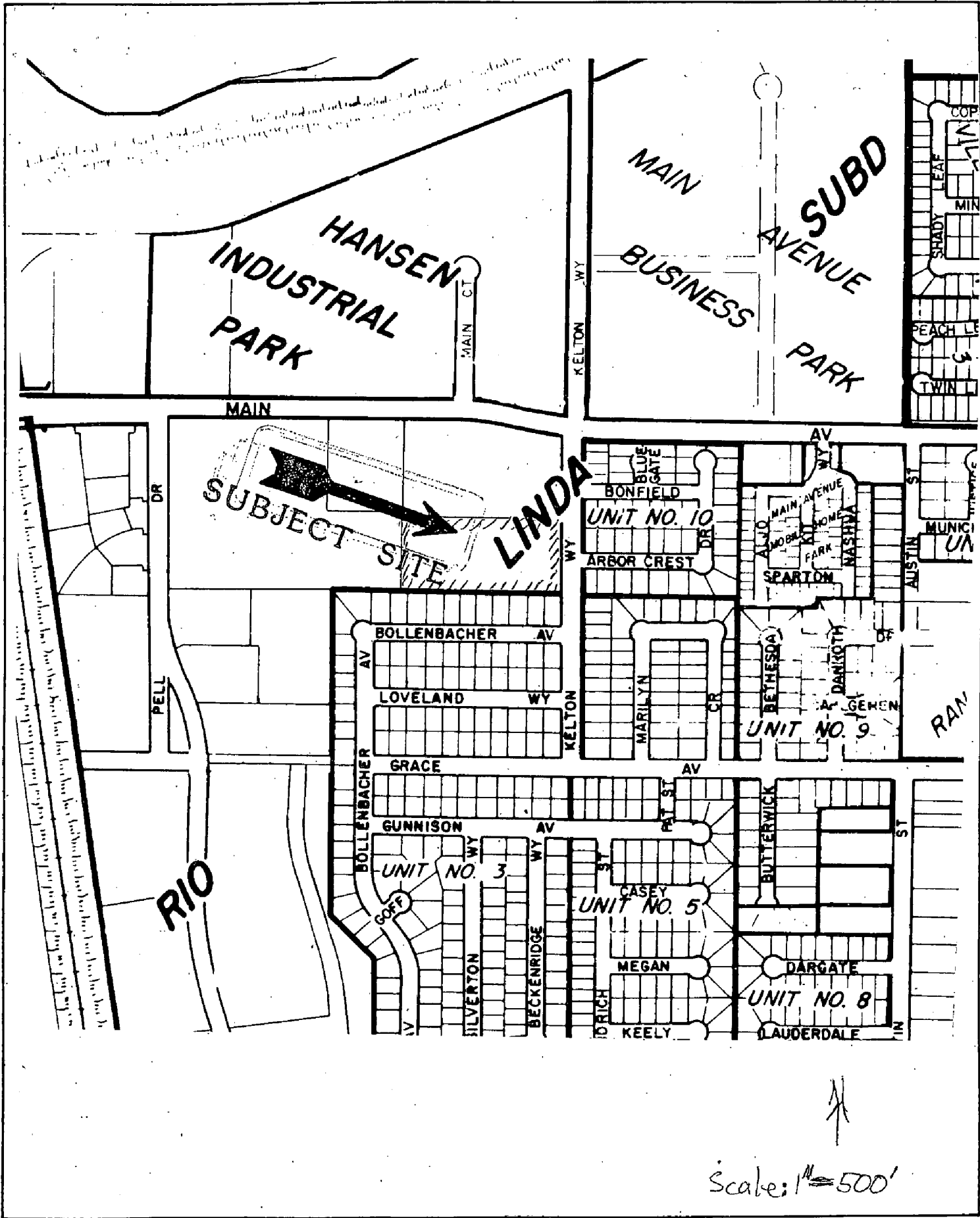
permitted.

1. If an individual building has more than one tenant/occupant, each occupant is permitted one attached sign not exceeding two feet in height and 30 square feet in area.
 2. Each occupant shall be permitted a 2' x 2' (maximum area four square feet) directional/identification sign at each of their respective loading dock(s) if located on a side of the building without an identification sign.
30. All light poles shall not exceed 20 feet in height and contain boxed lamps so that illumination is confined to the project site.
 31. No exterior loudspeaker system is allowed. Interior paging systems are allowed so long as they are not audible outside the building.
 32. The parking lot redesign shall show tree finger wells where 15 or more consecutive parking spaces are located. The parking area shall comply with the City Tree Shading Ordinance.
 33. A wing wall 10 feet high, 60 feet long shall be constructed along Building A's east dock area.
 34. *Additional evergreen trees along the south property line between the buildings and existing homes shall be installed at a density of one tree per 10 lineal feet. (CPC added)*
 35. *The eastern exterior wall that faces Kelton Avenue and the end panels on the north and south side shall be exposed aggregate. (CPC added)*
 36. *The applicant shall eliminate the driveway access onto Kelton Avenue and provide access from Main Avenue if the applicant or the local residences are able to obtain an easement for ingress and egress across the property to the north from Main Avenue. (CPC added)*

Findings of Fact - Plan Review

1. The project, as conditioned, is based upon sound principles of land use, in that:
 - a. Revised building orientation and elevation enhancements will be more compatible with the newer industrial development of the area and residential uses to the east and south;
 - b. Intensive landscaping, walls, designs and on-site lighting will lessen the visual impact on the residential area;

2. The project, as conditioned, will not be injurious or detrimental to public health, safety or welfare or result in the creation of a nuisance in that adequate on-site parking will be provided.
3. The proposed project is found to be consistent with the City's Discretionary Land Use Policy based on review of the 1984 North Sacramento Community Plan which designates the site for industrial use and the proposed warehouse/office project conforms with the plan designation.



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VICINITY MAP

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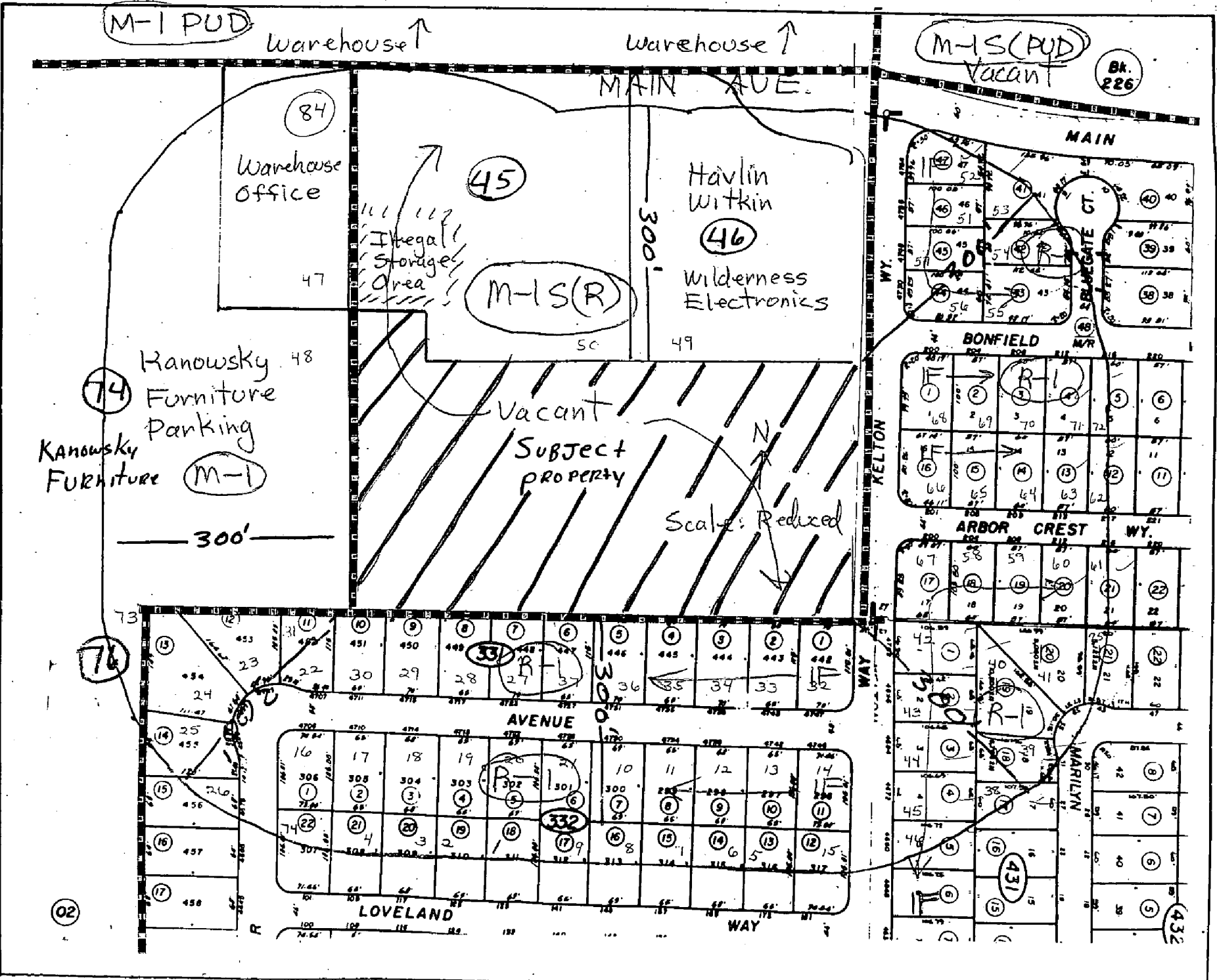
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LAND USE & ZONING MAP 001809

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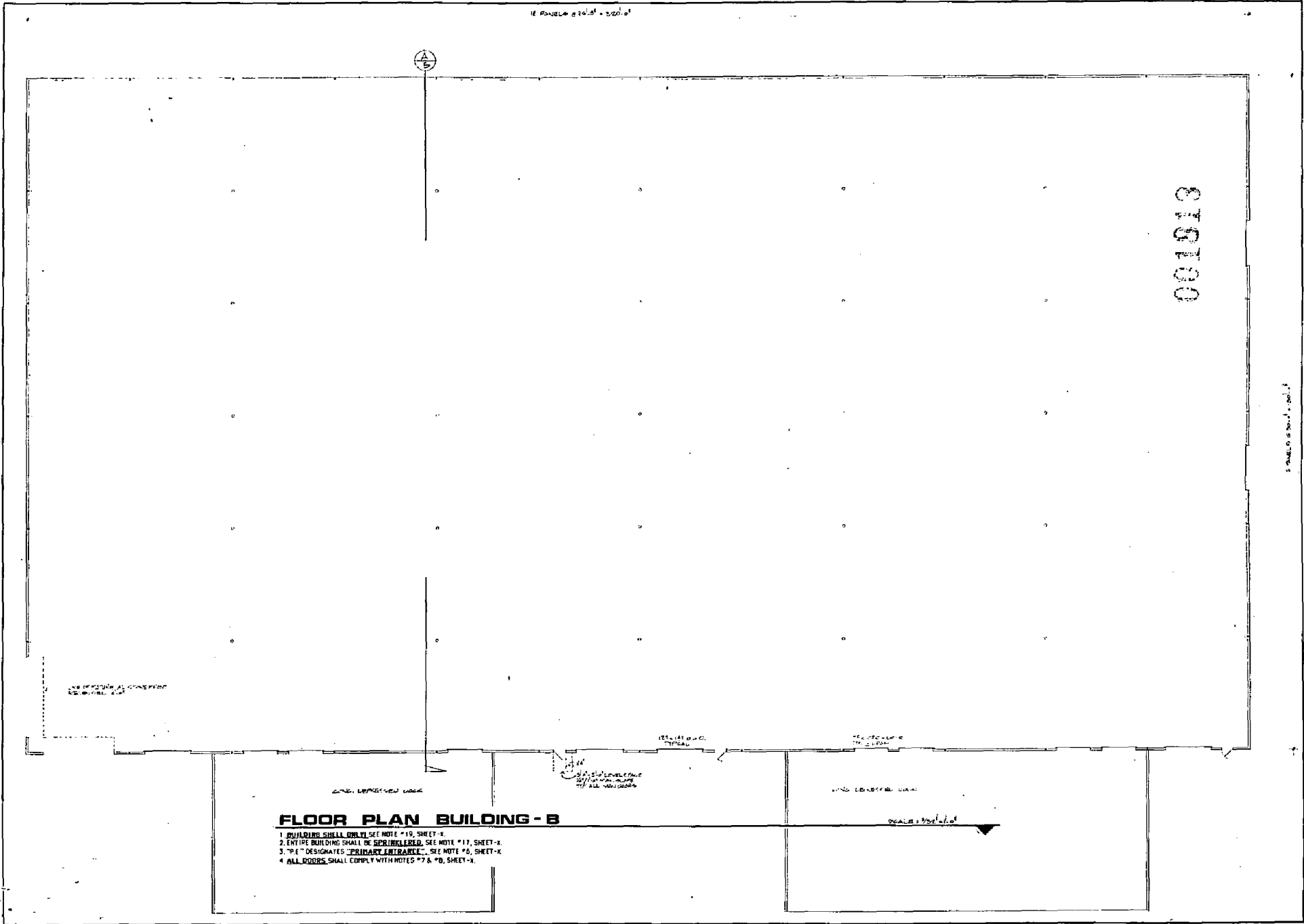
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16 PANELS @ 10' x 10' = 1,600'²

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FLOOR PLAN BUILDING - B

- 1. BUILDING SHELL ONLY SEE NOTE #19, SHEET-X
- 2. ENTIRE BUILDING SHALL BE SPRINKLERED, SEE NOTE #17, SHEET-X
- 3. "P" DESIGNATES "PRIMARY ENTRANCE", SEE NOTE #6, SHEET-X
- 4. ALL DOORS SHALL COMPLY WITH NOTES #7 & #8, SHEET-X

REVISIONS	BY

Leo McGlade & Associates, Inc.
 2177 NORTH 30TH STREET, SUITE 1
 SACRAMENTO, CA 95833 (916) 485-8800

BUILDING - B

2nd STORAGE BUILDINGS
 KELTON WAY, SOUTH OF MAIN AVE.
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

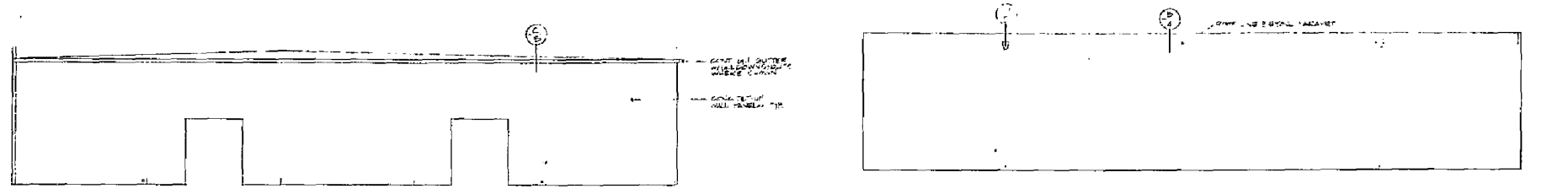
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ELEVATIONS

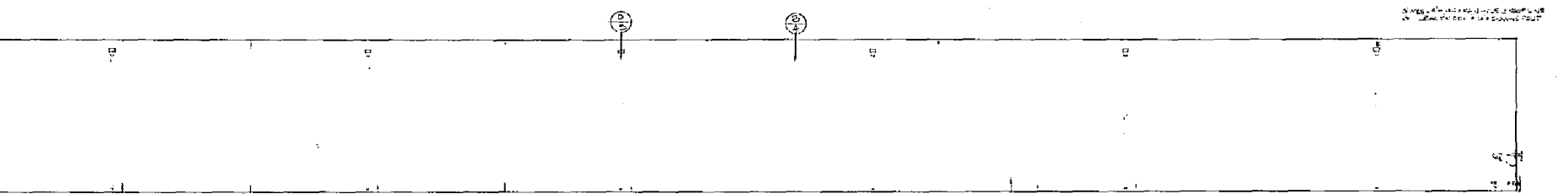


EAST ELEVATION
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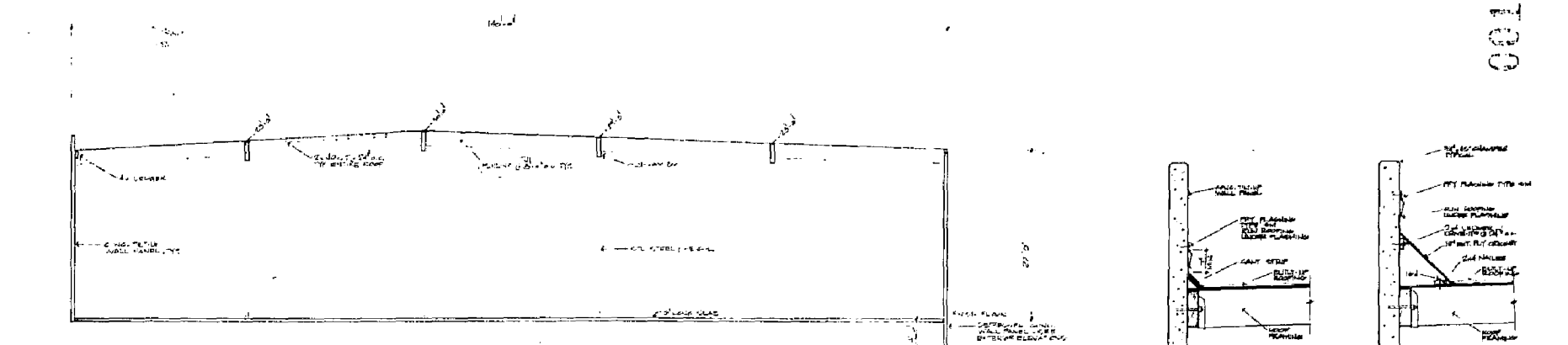


SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

NORTH ELEVATION
SCALE: 1/8" = 1'-0"

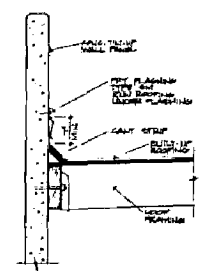


WEST ELEVATION
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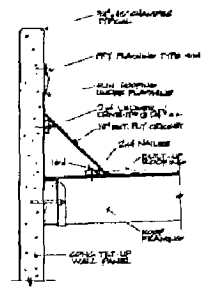


CROSS SECTION
SCALE: 1/4" = 1'-0"

(A)
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SECTION B-B
SCALE: 1/4" = 1'-0"



SECTION C-C
SCALE: 1/4" = 1'-0"

REVISIONS	BY

Leo McGlade & Associates, Inc.
Architects
1111 Northgate Blvd., Sacramento, CA 95833
916-442-2200

BUILDING - A

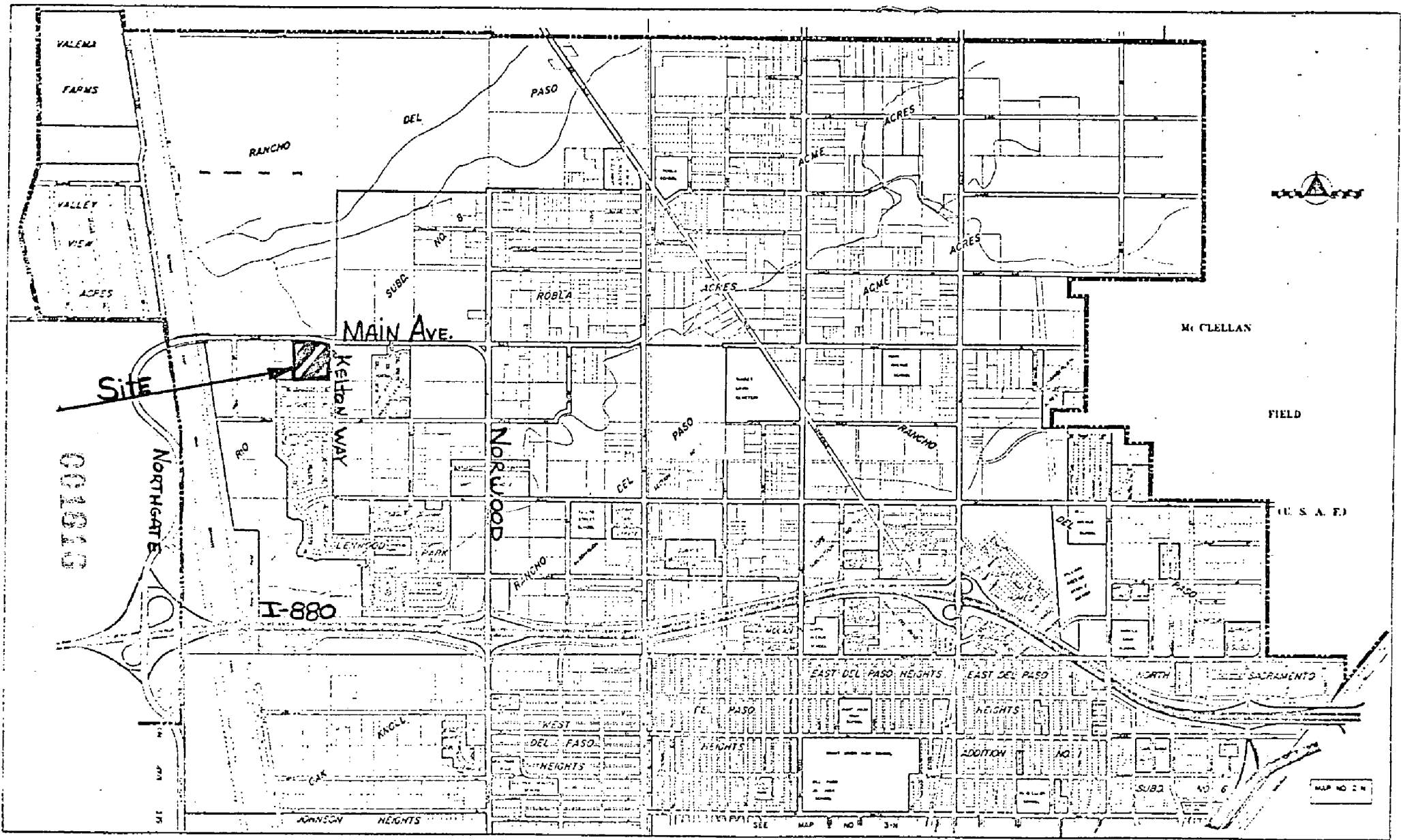
2nd STORAGE BUILDINGS
KELTON WAY, SOUTH OF MAIN AVE.
COUNTY OF SACRAMENTO, CA.
CARL PANATTONI - DEVELOPER

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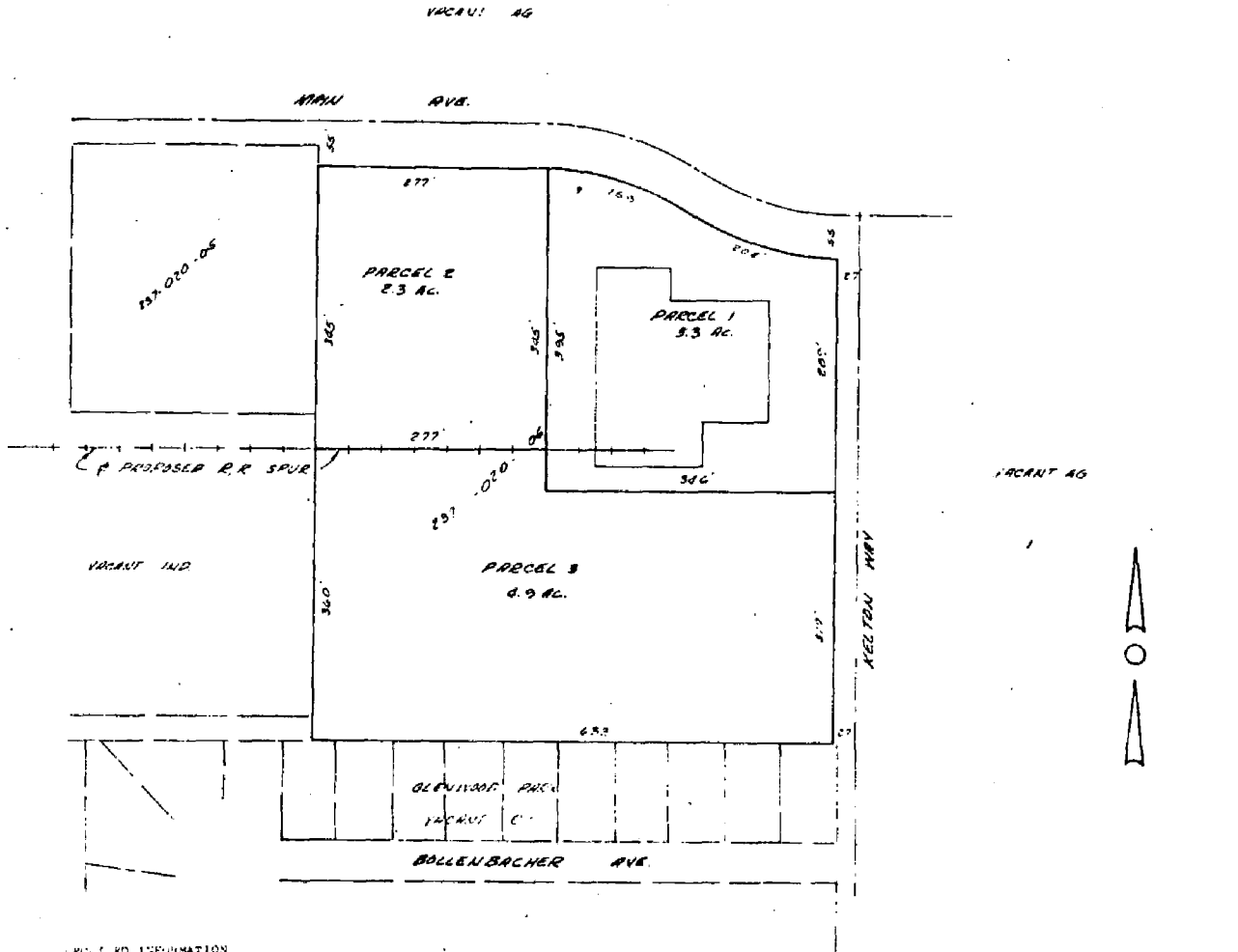
TENTATIVE PARCEL MAP

Lot 27, RIO LINDA SUB. 8

CITY OF SACRAMENTO, CALIFORNIA

NOVEMBER, 1974

SCALE 1"=100'



EXISTING INFORMATION

USES

- Present - Vacant
- Proposed - Parcel 1 Warehouse building to be built immediately.
- Parcel 2 Tax, vegetation & landscaping.
- Parcel 3 Future industrial buildings.

PUBLIC UTILITIES

Water, sewer and storm drain exist on or adjacent to the property.

PUBLIC STREET IMPROVEMENTS

To be installed with On-Site improvements.

ADJACENT LAND USE

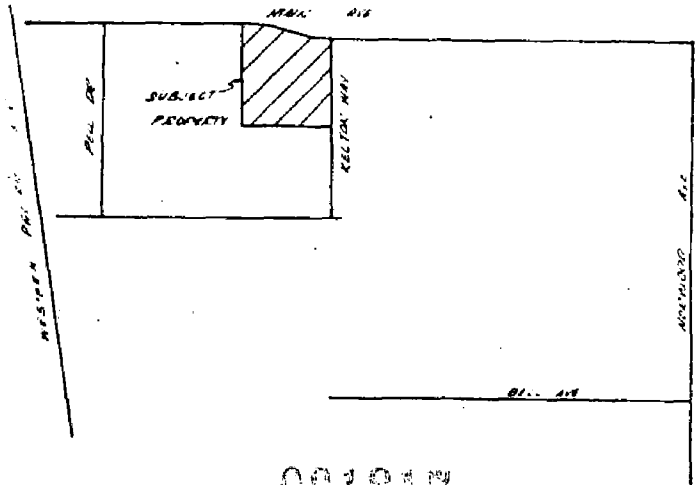
- South Residential with no homes built.
- West Vacant industrial
- No. West Taxon
- North Vacant agricultural
- East Vacant agricultural

ACRES

Overall	10.5 Ac. Net
Parcel 1	3.3 Ac. Net
Parcel 2	2.3 Ac. Net
Parcel 3	0.9 Ac. Net

GRADE PLANE AREA

Parcel 1	41,250
Parcel 2	Vacant
Parcel 3	Vacant



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DRAWN

DAVID WILKIN
4151 Jotway Court
No. Highlands, CA.

SUPERVISOR ENGINEER

William A. Arnold
2426 Glenlake Lane
Sacramento, CA 95825

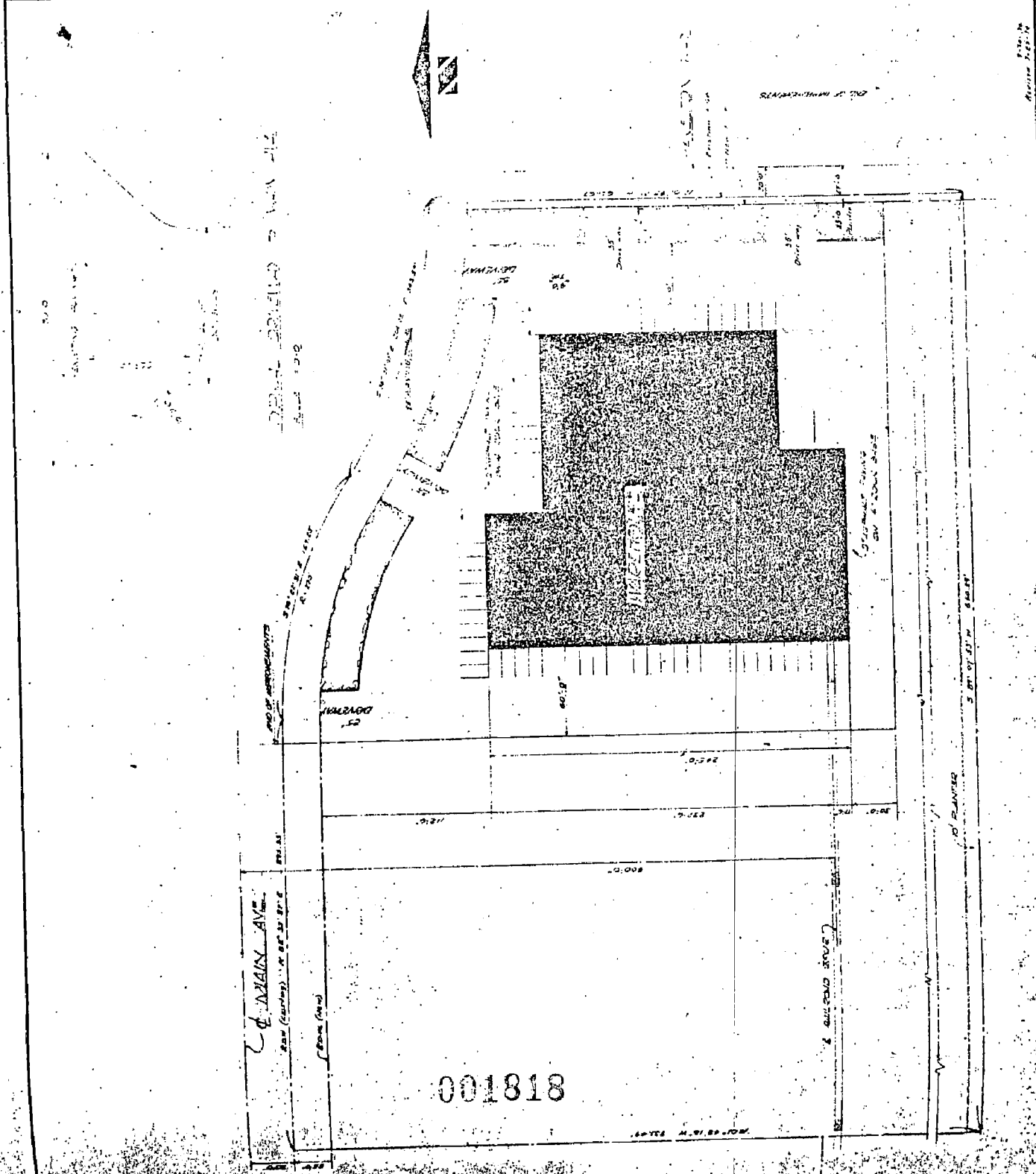
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LEO MCLAIDE, ENGINEER
1000 CALIFORNIA LANE - SACRAMENTO, CALIF. 95811
C.E. 2110

2-DOT PLAN

RESERVE CASE FOR
TAXIN WITHIN SACRAMENTO
MAY 1981 & KESTON WAY

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001818

Scale 1/8" = 1'-0"

EXHIBIT A STAFF MODIFIED SITE PLAN

- ① Add 6 ft high solid decorative masonry wall.
- ② Add 25 ft wide landscape strip along South property line.

- ④ Add 10 ft. landscape planter strip along North property line - Reduce planter width to 6 ft. opposite truck loading bay's.

- ⑤ Add 6 ft high decorative wrought iron fence along North property line

- ⑦ Construct 6 ft chainlink fence behind Havlin-Witkin setback

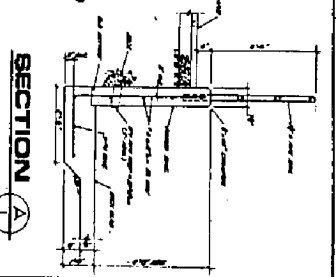
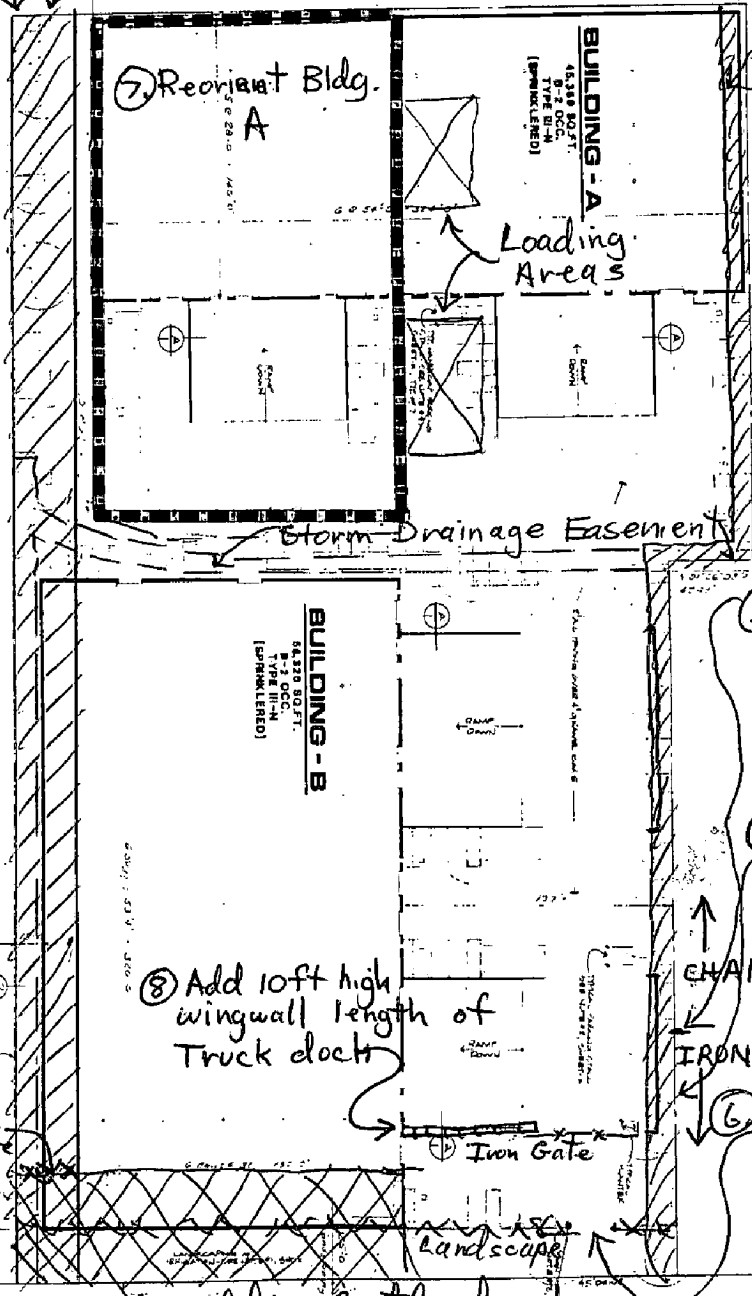
- ⑧ Add 10 ft high wingwall length of Truck dock

- ⑥ Add wrought-iron security gate roll type & Fence along Kelton behind 25 ft Setback on along wingwall

- ③ Increase Building Setback to 50 feet - Minimum of 25 feet landscaping from Kelton Way or reduce height of building to 12 to 15 feet at 25 ft. setback and increase to 29 ft. height at 50 ft. setback.

SITE PLAN

FOR HANDICAP "PATH OF TRAVEL" REQUIREMENTS, SEE NOTE #5, SHEET X



PARKING
 20 SPACES
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'
 20' x 10' x 10'

2nd STORAGE BUILDINGS
 KELTON WAY, SOUTH OF MAIN AVE.
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

SITE PLAN

M Leo McGlade & associates, Inc.
 1517 Archer Way, Suite A, Sacramento, CA 95825 (916) 488-8300

DATE	DESCRIPTION

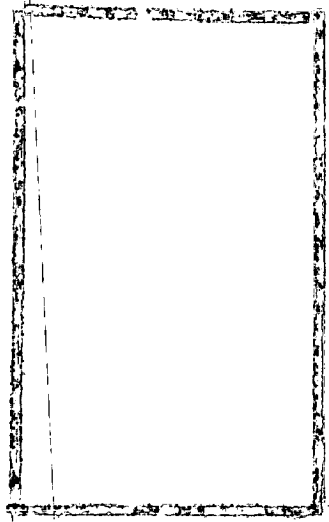


EXHIBIT B
CORRESPONDANCE
CITY PLANNING DIVISION

TO: Will Weitman, Senior Planner
Sacramento City Planning Department
1231 "I" Street, Suite 200
Sacramento, California 95814

AUG 5 1987

FROM: The Meadows Development Association
c/o Arek Firman
157 Gunnison Avenue
Sacramento, California 95838

RECEIVED

SUBJECT: P-87-292 -- Proposed Office and Warehouse Shell on
West side Kelton Way, 300 Feet South of Main Avenue

DATE: August 4, 1987

As a spokes person for the Meadows Development Association, I am submitting this letter to voice our increasing concerns at the efforts to further detract and negatively impact our residential neighborhood by the continued approvals made on commercial/industrial developments such as the proposed project to be located at the southwest corner of Kelton Way and Main Avenue in North Sacramento. It is our hope that by making the Planning Commission aware of the issues most important to us, key actions will be taken by the Commission to minimize the potentially negative outgrowth of commercial development in our area.

We would like to see the following changes made in this proposal:

- 1) Reduce office warehouse buildings to four (4) 20,000 square feet units, with a maximum height of 20 feet and architecturally compatiabile with the residential community.
- 2) A 88 feet building setback off Kelton Way and a 25 feet from the southern property line (same building setback as Havlin Watkin).
- 3) Exposed aggregate surface with design variations.
- 4) On Kelton Way, frontage the use of a variety of surfaces (stone, brick, and wood) with windows, canopies, and awnings to establish architectural continuity with the residential neighborhood.
- 5) No access of Kelton Way. The street is for residential use only. All access is to be off Main Avenue. Additionally, the developer should improve the Main Avenue and Kelton Way intersection and incur all costs for sidewalks and curbs.

P-87-292

9-24-87

~~8-13-87~~ ~~9-10-87~~
8-27-87

item $\frac{14}{95}$

Will Weitman, Senior Planner
Sacramento City Planning Department
RE: P-87-292
August 4, 1987
Page Two

- 6) Put monies into a Trust Fund for signal lights at the intersection of Kelton Way and Main Avenue.
- 7) Landscape a 25' wide strip with four (4) feet high beams and 15 gallon trees on 10 feet centers along the Kelton Way and southern property line. Also, buildings are to be surrounded by landscaping.
- 8) Restrict tenants to uses which do not produce noises or emissions outside the buildings (no woodworking, metal fabrication, or assembly shops).
- 9) All lighting to be directed on site and not to reflect off-site into the residential area.
- 10) Restrict hours of operations from sunrise to sunset or 7 a.m. to 8 p.m. for all tenants.
- 11) Require all loading and receiving docks and doors to face away from residential area.
- 12) Install a 6 feet high wrought iron fence along Kelton Way to the north property line to prevent unauthorized access.
- 13) Install a 8 feet high block wall to act as a sound barrier.
- 14) No public address systems outdoors.
- 15) Residencies requirement of 50% of the total employees for the entire site.
- 16) Require the developer to work with a community committee in establishing landscaping and architectural designs.

P-87-292

9-24-87 ~~8-13-87~~ 9-10-87
~~8-27-87~~

item #5
9

CITY PLANNING COMMISSION

1231 "I" STREET, SUITE 200, SACRAMENTO, CA 95814

APPLICANT	Julie Arnold, 7728 Wilbur Way, Sacramento, CA 95828		
OWNER	Carl Panattoni, 7728 Wilbur Way, Sacramento, CA 95828		
PLANS BY	Leo McGlade Associates		
FILING DATE	6-30-87	ENVIR. DET.	Cat. Ex. 1506(b)(3) REPORT BY DJH:tc
ASSESSOR'S-PCL. NO.	237-022-044		

APPLICATION: Plan Review to allow construction of two warehouse/office buildings totaling 103,680+ square feet on 5.03+ vacant acres in the Light-Industrial-Review (M-1(S)-R)

LOCATION: West side Kelton Way 300+ feet south of Main Avenue

PROPOSAL: The applicant is requesting the necessary entitlements to construct two warehouse buildings of 45,360+ square feet and 58,320+ square feet each.

PROJECT INFORMATION:

1974 General Plan Designation: Industrial
1984 North Sacramento Community
Plan Designation: Industrial
Existing Zoning of Site: M-1(S)R
Existing Land Use of Site: Vacant

Surrounding Land Use and Zoning:	Setbacks:	Required	Provided
North: Warehouse/Office & Vacant; M-1(S)R	Front:	25 ft.	25 ft.
South: Single Family; R-1	Side(Int):	5 ft.	13 ft.
East: Single Family; R-1	Side(St):	0 ft.	0 ft.
West: Furniture Mnft; M-1(S)R	Rear:	0 ft.	5 ft.

Parking Required: 135 spaces @ 20% Office and 80% warehouse
Parking Provided: 135 spaces
Property Dimensions: 633' x 364'
Property Area: 5.03+ acres
Square Footage of Building: Office = 20,736 sq. ft.;
Warehouse = 82,944 sq. ft.;
Total = 103,680 sq. ft.
Height of Building: 29 ft., one-story
Topography: Flat
Street Improvements: Existing
Utilities: Existing
Exterior Building Materials: Concrete tilt-up
Roof Material: Tar
Estimated Number of Employees: 153

Project History:

In 1971 the site was approved for a rezone from Agriculture to Light Industrial Park (M-1S-R) zone uses were allowed.

Project Evaluation: Staff has the following comments:

A. Land Use and Zoning:

The subject site contains 5.03+ vacant acres in the Light Industrial-Review (M-1(S)R) zone. The 1984 North Sacramento Community Plan designates the site as Industrial. Adjacent land uses include single family residential to the south and east, a furniture manufacturer and warehouse to the west, and warehouse and truck terminal to the north across Main Avenue with a window manufacturer and electronics assembly plant immediately north of the site.

The site is subject to Plan Review by the Planning Commission and is required to have a 25 foot landscape setback along public street frontages.

The subject site is currently vacant with a storm drain sewer line and easement bisecting the site. Street improvements of curb, gutter and sidewalks are installed along Kelton Way.

B. Site Plan Evaluation:

Staff has several concerns over the proposed project which will require site plan modification. (Refer to Exhibit A, staff modified site plan).

1. Setbacks: The proposed buildings are very massive with 29 feet of height. In order to buffer the wall effect from the residential properties to the south, staff recommends increasing the setback along the south property line to 25 feet from the proposed 13 foot setback for building B.. Staff also recommends extensive landscaping with evergreen trees on 20 foot centers consisting of redwood and pine trees. Groundcover should be turf with a walkway and employee break areas established. Trees shall a minimum of five gallon size. The 25 foot landscape strip shall run along the entire south property line.

The building setback along Kelton is proposed at 25 feet. Staff observes that the Havlin - Witkin building is setback approximately 88 feet from the setback edge. Staff recommends that building B be setback 50 feet or more. The applicant suggested that in order to maintain a 25 foot building setback along Kelton Way, that he is willing to reduce the height from 29 feet to between 10 and 15 feet for the storefront window panels and install a sloping roof back to the 50 foot setback line where the height of the wall would be 29 feet. The revised site plan and elevations should show one of the two alternatives.

Staff recommends landscaping in the front setback area a minimum of 25 feet in width with three to four foot high undulating berms. Intensive tree and shrubbery are to be provided with lawn ground cover. Trees shall include a mix of five and 15 gallon and 24 inch boxed specimen tree sizes on centers of 15 feet as a minimum. Clustering and

accenting trees is recommended with at least 50 percent of the trees along Kelton Way as evergreen trees. All landscaped areas are to have automatic irrigation.

2. Wall Requirement: The Zoning Ordinance requires a minimum six foot high solid masonry wall separating residential from non-residential uses. Staff field inspected the site and noted that Kanowsky Furniture, locate due west of the site has an eight foot high solid wall. The wall is located at the original base grade elevation with the parking lot finish grade elevation nearly four feet higher so that the effective wall height is only four feet. Staff recommends that the new six foot high wall be constructed at the top of the fill slope or on top of a retaining wall. The six foot height of the wall shall be measured from the warehouse side grade elevation. The wall of the building will also provide a buffer for any potential noise from the north side of the building.
3. Reorient Building A: Building A has two proposed below grade loading docks which will generate noise to the residences to the south. Staff recommends that building A be reoriented so that the loading bays face to the north. The building will be required to observe the storm drainage easement. Landscaping is recommended between the two buildings. In turning the building the rear wall will serve as a noise barrier.
4. Landscape - North Property Line: Staff reviewed the landscaping and recommends a 10 foot wide landscape strip along the north property line to be planted with trees, shrubbery and living ground cover. A six foot high decorative wrought iron fence is recommended to provide security and an aesthetically pleasing transition to the residential area along Kelton Way and up to the Havlin Witkin setback. Chain link fencing is appropriate for the remaining north fence line. Parking along the north property line shall include a two foot vehicle overhang into the landscape strip. No portable concrete wheel stops will be allowed. All landscape areas are to be bordered by a six inch raised poured in place concrete curb. Razor ribbon wire shall not be used. Due to the truck loading dock locations opposite a row of parking along the north property line, the normal 105 foot distance required for a semitruck-trailer combination movement would be reduced to 99 feet. Staff recommends the total landscape width along the north property line opposite Building B's loading docks be six feet in width and that the spaces be reserved for compact cars only. This would maintain the minimum 105 feet for vehicle maneuvering with the repositioning of Building B to the north by 12 feet. Refer to Exhibit A for the location of the reduced landscape planter.
5. Parking: A total of 135 parking spaces are shown on the proposed site plan based upon a maximum of 20 percent office and 80 percent warehouse use.

Office Area = 20,736 square feet @ one space per 400' = 52 spaces
 Warehouse = 82,944 square feet @ one space per 1,000 square feet = 83 spaces
 Total = 1,355 spaces

City Ordinance allows up to 25 percent office use in conjunction with a warehouse use by right. Office use over 25 percent requires a Planning Commission Special Permit. Any change in square footage or use mix will require modification to the Plan Review. All surface parking areas and maneuvering areas are required to meet the minimum 50 percent shading requirement.

6. Lighting: No detailed lighting was provided for staff review. Staff recommends that all lighting be directed on-site and not reflect off-site. Use of low level pole lights and boxed fixtures shall be included for building permit review. Lamp poles shall not exceed 20 feet in height. Any lighting proposed along the south property line or west property line shall not glare onto residentially zoned property to the south.
7. Exterior Loudspeakers: Problems with exterior public address systems have developed along Pell Drive for residents of Bollenbacher Avenue. Staff recommends that no external public address system be allowed for any tenant. Paging systems within each building are permissible so long as not audible from outside the building.
8. Parking Lot Fingers: In revising the site plan, staff recommends that no more than 15 consecutive parking spaces be shown without installation of tree well finger planters. The fingers will break up the long monotonous appearance of parking spaces without variation.
9. Trash Enclosure: The revised site plan shall include trash enclosures meeting trash enclosure guidelines. Trash enclosures are to be located more than 100 feet from residential uses.
10. Wingwall - Building B: Staff recommends that a 10 feet high solid masonry wing wall (decorative material) be constructed adjacent to the east side of the east loading dock on Building B for the 60 feet of length of the dock. The purpose of the wall is to deflect sounds from loading dock activity. Staff also recommends attaching a rolling gate constructed of decorative wrought iron to the wall and continue the fence to the north property line.

C. Elevation Evaluation:

Staff has major objections to the proposed project's elevations. Due to the proximity to single family dwellings, the stark contrast between two, 50,000 square foot warehouse buildings and residences is drastic. Staff proposes

the following changes in order to lessen the impact such large massive buildings have on adjacent residential uses. Overall, staff would prefer the total redesign of the project to smaller commercial buildings similar to those being built along Pell Drive in the City and North Market and Northgate Boulevard in the County.

1. Building B - East Elevation.

The 180 feet of Building B facing Kelton Way is stark, bland and uninteresting. Staff recommend the applicant redesign the east elevation to have more three dimensional relief by adding store front windows and either project or recess them with use of awning or canopy feature so that the wall has movement and interest. Windows shall be reviewed and approved by the design review coordinator.

Staff also notes that no relief is proposed on the concrete panels. Staff recommends the applicant incorporate a two to three foot high formliner along the top elevation of each panel with two, three inch wide, one inch deep reveal lines. The formliner and reveal bands are to circle the buildings. In addition, all walls facing the south property line shall include vertical bands or relief features at each joint. The vertical feature may be painted a trim color compatible with the overall project.

2. Colors:

No colors were indicated by the applicant. Staff recommends that elevations visible from Kelton Way be a sandblasted exposed aggregate surface similar to the buildings constructed at 110 and 120 Main Avenue. Paint is allowed on the window trim and accent trim. The roll-up doors and entry doors shall be painted a color to match the building. the color scheme for both buildings shall be reviewed and approved by the Planning Director. Colors shall be earth tone and be compatible with the colors present on the single family dwellings east across Kelton Way and to the south along Bollenbacher. Staff recommends a sandblasted concrete finish on all exterior surfaces rather than painted walls.

D. Tenants:

No specific tenants are identified for use of the buildings. The Zoning Ordinance allows a variety of uses in the M-1 zone which may create potential conflicts with the residences in the area. Staff recommends that the applicant record in lease arrangements that no activity is to occur where noise, dust or emissions are generated which violate noise or air quality standards. No outdoor storage of materials is to occur which is not screened.

Staff also recommends that the hours of operation of the facility be restricted to 6 a.m. to 9 p.m. Monday through Friday and 8 a.m. to 6 p.m. on

Saturday and Sunday.

E. Off-Site Impacts:

1. Kelton Way Parking:

Tenants may use Kelton Way for parking. In order to minimize commercial impacts on the residential area, staff recommends that the applicant require future tenants to not park trucks or automobiles on Kelton Way. The street shall be posted "No Overnight Parking" adjacent to the project site. All trucks are to be parked on-site.

2. Kelton Way and Main Avenue Intersection:

Staff has received a center expressing concerns regarding the level of improvements at the Kelton - Main Avenue intersection. They feel the project will create an adverse traffic situation if 153 employees and increased truck traffic is added to the intersection. Area residents are concerned that the City has not improved the intersection to be safer. The City Traffic Engineer commented that the intersection is not operating at an unsafe level and that the design is adequate.

F. Signage:

The applicant has not submitted any signage for the project. Staff recommends that applicant submit a detailed sign program for the entire site. Staff recommends one monument identification sign with no internal illumination. The monument sign shall e a maximum height of six feet. Any storefront signs facing Kelton Way shall not be illuminated. All other signs shall comply with the City Sign Ordinance.

G. Agency Review:

The proposed project was reviewed by the City Engineer, Traffic Engineer, Building Inspection and Meadows Development Association with comments received as noted. The Meadows Development Association has contacted staff and has submitted a letter (Exhibit B) and petition prior to the Commission hearing.

The City Engineers office commented that necessary on-site drainage would be required and that a minimum of 10 feet shall be shown for the driveway off Kelton from the property line to the edge of the driveway.

ENVIRONMENTAL DETERMINATION: The proposed project is exempt from environmental review pursuant to STate EIR Guidelines (CEQA, Section 15061(b)(3)).

STAFF RECOMMENDATION: Staff recommends that the Commission take the following actions:

Approve the Plan Review subject to conditions and based upon findings of

fact which follow:

Conditions: Plan Review

1. The applicant shall prepare a revised site plan, landscaping and irrigation plan for review and approval by the Planning Director prior to issuance of building permits (Refer to Exhibit A).
2. The applicant shall revise the exterior elevations to the satisfaction of the Planning Director and Design Review Coordinator. All colors and material are to be included in the revised elevations.
3. Future tenants shall be informed in their lease arrangements that no activity is to occur where noise, dust or emissions are generated.
4. Every use, activity, and process shall be so operated that regularly recurring noises are not disturbing or unreasonably loud, and do not cause injury, detriment, or nuisance to any person. Every use, activity and process in business and industrial areas shall be so operated that regularly recurring noises, as detected by the human sense of hearing, without instruments, at adjoining residential district boundary lines, shall not exceed the normal level generated by uses permitted in warehouse and office districts. All uses are to comply with the City Noise Ordinance.
5. Every use shall conform to the rules and regulations of the Air Pollution Control District, County of Sacramento, and these regulations, and shall serve as minimum guidelines for determining permissible emissions. No outdoor storage of materials is to occur which is not screened.
6. Hours of operation shall be restricted to be from 6 a.m. to 9 p.m. Monday through Friday and 8 a.m. to 6 p.m. on Saturday and Sunday. No delivery or work is to occur during the evening hours.
7. All tenants shall be informed that no parking is allowed on Kelton Way for either cars or trucks. All vehicles are to be parked on the premises. Conditions 3 and 5 are to be recorded in all lease agreements. The Planning staff shall review and approve the wording in the lease agreement prior to issuance of building permits.
8. The applicant shall submit a uniform sign program for the review and approval by the Planning Director prior to issuance of any sign permits.
9. Kelton Way street frontage is to be posted "No Overnight Parking".
10. Lighting design shall be such as not to produce a hazardous and annoying glare to motorists, building occupants, or to the general public.

11. Lighting is to be oriented away from the properties adjacent to the site.
12. Minimum parking dimensions shall correspond to the City Zoning Ordinance, except that the front two feet of all stalls, the areas into which the vehicle bumper overhangs, shall be incorporated into adjacent landscape or walkway improvements. No individual prefabricated wheel stop will be permitted. A continuous six inch raised poured in-place concrete curb shall be provided along all landscape areas abutting parking or drives.
13. A minimum 50 foot street setback from building shall be provided on Kelton Way if building height remains 29 feet.
14. Building height shall be limited to 29 feet.
15. Building setback from residential areas shall be 25 feet.
16. The Kelton Way setback may be decreased to within 25 feet if the height is reduced to between 10 to 15 feet from the proposed 29 feet.
17. Temporary structures will only be permitted if they are attendant to the construction of a permanent building and shall be placed at the start of construction and removed at completion of construction.
18. Such structures shall be as inconspicuous as possible.
19. Large items such as air conditioning, ventilating, or other mechanical equipment shall be screened or enclosed in such a manner as to hide such equipment. The design and material of the screening shall be compatible with the building architecture.
20. The projections shall be painted to match the roof or building.
21. All mechanical equipment, utility meters and storage tanks shall be screened and architecturally designed to be an integral part of the building.
22. Penthouses and mechanical equipment screening shall be of a design and material similar to and compatible with those used in the buildings.
23. Underground utility services throughout the project will be required.
24. All mechanical equipment shall be located in such a manner so as not to cause a nuisance or discomfort from noise, fumes, odors, etc.
25. Finish building materials shall be applied to all sides of a building which are visible to the general public and the occupants of other buildings.

26. Untextured concrete block exposed to the exterior or large surfaces of untextured tilt-up concrete panels shall not be acceptable unless approved by the City of Sacramento Planning Department.
27. The general overall atmosphere of color shall be natural tones. Wood, natural stone, brick and dark anodized aluminum finishes, etc., shall be the background colors. Accent colors shall be used whenever necessary.
28. GARAGE, LOADING DOCK, AND OTHER SERVICES AREAS

These elements shall be so located as to cause no nuisance to the general public or occupants of the other buildings.

- a. The walls of the trash enclosure structure shall be constructed of solid masonry material with decorative exterior surface finish compatible to the main structure. Split face concrete block finish is recommended. The decorative finish is only required on walls facing the public street.
- b. The trash enclosure structure shall have decorative heavy gauge metal gates and be designed with cane bolts on the doors to secure the gates when in the open position.
- c. The trash enclosure facility shall be designed to allow walk-in access without having to open the main enclosure gates.
- d. The walls shall be minimum six feet in height, more if necessary for adequate screening.
- e. The perimeter of the trash enclosure structure shall be planted with landscaping, including a combination of shrubs and/or climbing evergreen vines.
- f. A concrete apron shall be constructed either in front of the trash enclosure facility or at point of dumpster pickup by the waste removal truck. The location, size and orientation of the concrete apron shall depend on the design capacity of the trash enclosure facility (number of trash dumpsters provided) and the direction of the waste removal truck at point of dumpster pickup.

The minimum dimensions of the concrete apron for a single, two cubic yard dumpster shall be: width 10 feet or width of enclosure facility; length 20 feet. Larger trash enclosure facilities shall require a larger concrete apron, subject to the approval of the City Building Inspections Building Technicians (Plan Checker).

Paving material shall consist of 5 inches aggregate base rock and six inches portland cement paving.

- g. The trash enclosure shall be located a minimum of 100 feet from any residential zone or use. No unenclosed trash bins are allowed. No outdoor storage of refuse is allowed.
 - h. The enclosure shall be adequate in capacity, number and distribution.
29. The following Sign Regulations shall be reflected in preparation of a uniform signage program for the site.

SIGN REGULATIONS:

- a. A sign program shall be submitted to the City Planning staff if submitted subsequent to the City Planning Commission special permit hearing.
- b. Flashing, moving, or audible signs will not be permitted.
- c. In no case shall the wording of signs describe the products sold, prices, or any type of advertising except as part of the occupant's trade name or insignia.
- d. No signs shall be permitted on canopy roofs or building roofs.
- e. No sign or any portion thereof shall project above the building or top of the wall upon which it is mounted.
- f. All attached building signs shall be placed flat against the building.
- g. Freestanding pole signs shall not be permitted with the exception of one monument sign.
- h. No sign shall be specifically designed or oriented to be viewed from Main Avenue.
- i. No signs perpendicular to the face of the building shall be permitted.
- j. No signs shall be attached to the Kelton Way frontage which are illuminated.

DESIGN REQUIREMENTS, SIGNS

- a. Signs shall be located only as shown on the approved schematic plan.
- b. All electrical signs shall bear the UL label and their installation must comply with all local building and electrical codes.

- c. No exposed conduit, tubing, or raceways will be permitted.
- d. No exposed neon lighting shall be used on signs, symbols, or decorative elements.
- e. All conductors, transformers, and other equipment shall be concealed.
- f. All signs, fastenings, bolts, and clips shall be of hot dipped galvanized iron, stainless steel, aluminum, brass or bronze.
- g. All exterior letters or signs exposed to the weather shall be mounted at least three fourth inch (3/4") from the building to permit property drainage.
- h. The location of all openings for conduit and sleeves in sign panels of building wall shall be indicated by the sign contractor on drawings submitted to the Design Review Coordinator. The installation shall be in accordance with the approved drawings.
- i. No signmakers' labels or other identification will be permitted on the exposed surface of signs, except those required by local ordinance which shall be located in an inconspicuous location.
- j. Painted lettering or signs shall not be permitted.

DESIGNATED PARK PROJECT IDENTIFICATION SIGN

- a. One non-illuminated monument sign as defined by Section 3.520 of the City Sign Ordinance shall be allowed for the Industrial Park.
- b. Maximum area of sign: 48 square feet.
- c. Maximum height of sign: 12 feet from street or parking lot grade, whichever is less.
- d. Location: to be located at the major entry off Kelton Way. The sign may be placed in the setback area; however, it must be located farther than 10 feet from the public right-of-way and from any driveway. No sign shall be allowed in the public right-of-way.

INDIVIDUAL BUSINESS/BUILDING SIGNS

One attached identification sign per building.

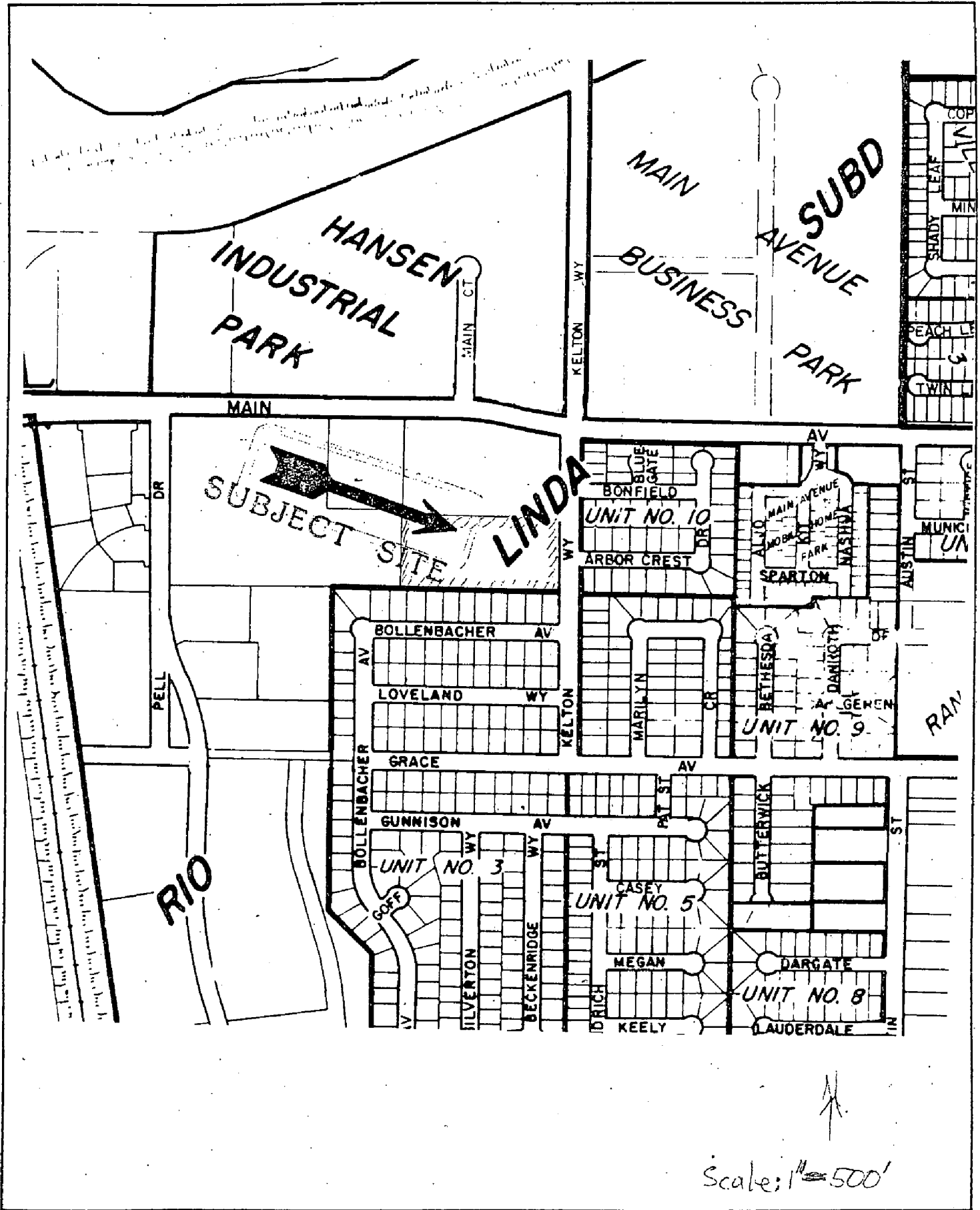
- a. Attached Sign: vertical height of sign or letters, including logo, shall not exceed two feet and overall size of sign shall not exceed 60 square feet. Each sign and business name shall consist of individual raised letter type. No canned plastic signs are

permitted.

1. If an individual building has more than one tenant/occupant, each occupant is permitted one attached sign not exceeding two feet in height and 30 square feet in area.
 2. Each occupant shall be permitted a 2' x 2' (maximum area four square feet) directional/identification sign at each of their respective loading dock(s) if located on a side of the building without an identification sign.
30. All light poles shall not exceed 20 feet in height and contain boxed lamps so that illumination is confined to the project site.
 31. No exterior loudspeaker system is allowed. Interior paging systems are allowed so long as they are not audible outside the building.
 32. The parking lot redesign shall show tree finger wells where 15 or more consecutive parking spaces are located. The parking area shall comply with the City Tree Shading Ordinance.
 33. A wing wall 10 feet high, 60 feet long shall be constructed along Building A's east dock area.

Findings of Fact - Plan Review

1. The project, as conditioned, is based upon sound principles of land use, in that:
 - a. Revised building orientation and elevation enhancements will be more compatible with the newer industrial development of the area and residential uses to the east and south;
 - b. Intensive landscaping, walls, designs and on-site lighting will lessen the visual impact on the residential area;
2. The project, as conditioned, will not be injurious or detrimental to public health, safety or welfare or result in the creation of a nuisance in that adequate on-site parking will be provided.
3. The proposed project is found to be consistent with the City's Discretionary Land Use Policy based on review of the 1984 North Sacramento Community Plan which designates the site for industrial use and the proposed warehouse/office project conforms with the plan designation.



VICINITY MAP

P-87-292

8-13-87

Stena II

LAND USE & ZONING MAP

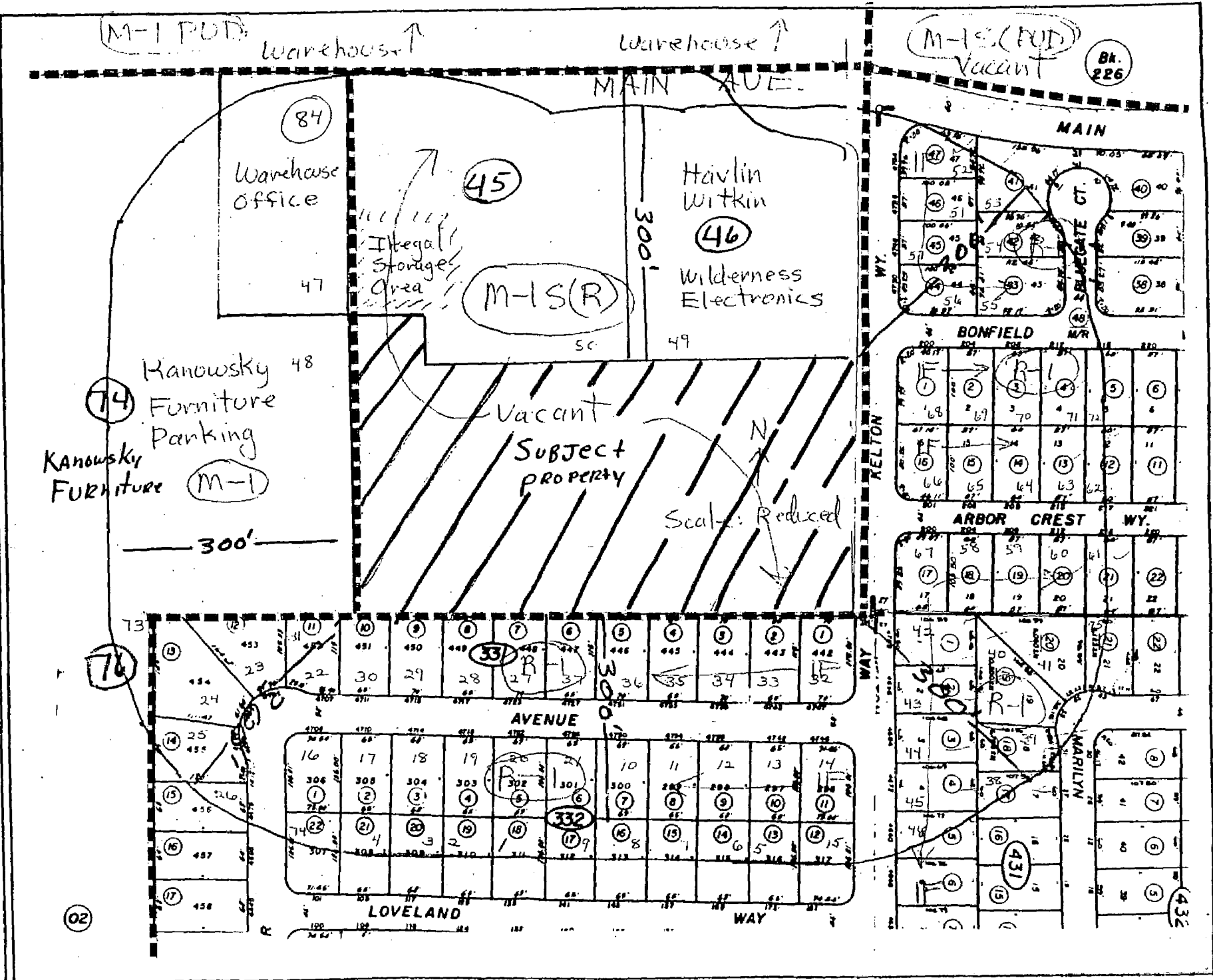
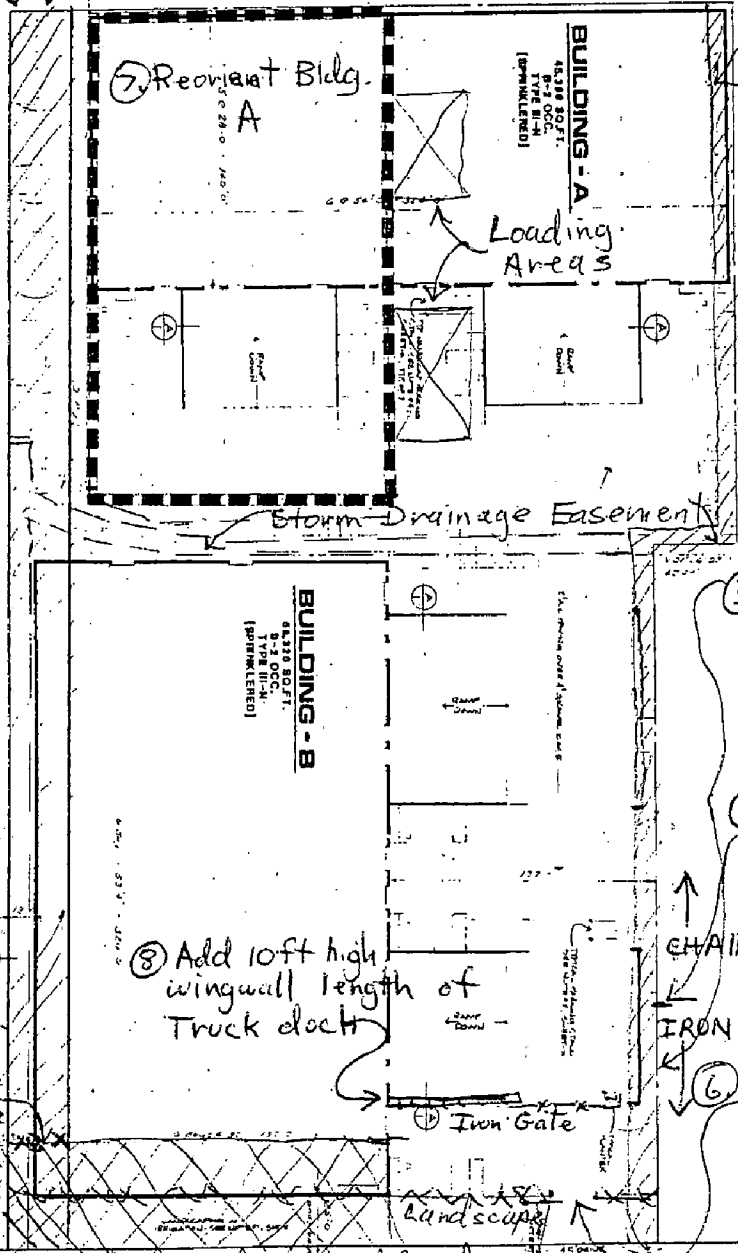


EXHIBIT A

STAFF MODIFIED SITE PLAN

- ① Add 6 ft high solid decorative masonry wall
- ② Add 25 ft wide landscape strip along South property line.

SITE PLAN
FOR HANDICAP PATH OF TRAVEL REQUIREMENTS, SEE NOTE # 6, SHEET X



- ④ Add 10 ft. landscape planter strip along North property line. - Reduce planter width to 6 ft. opposite truck loading bay's.

- ⑤ Add 6 ft high decorative wrought iron fence along North property line

- ⑦ Construct 6 ft chainlink fence behind Havlin - within setback

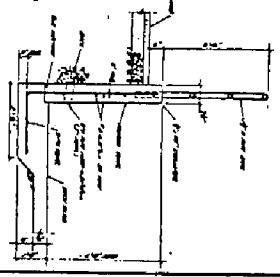
- ⑧ Add 10 ft high wingwall length of Truck dock

CHAINLINK
IRON FENCING

- ⑥ Add wrought-iron security gate + roll top fence along Kelton behind 25 ft setback or along wingwall

- ③ Increase Building Setback to 50 feet - Minimum of 25 feet landscaping from Kelton Way or reduce height of building to 12 ft or 15 feet at 25 ft setback and increase to 29 ft. height at 50 ft. setback.

SECTION



2- STORAGE BUILDINGS
KELTON WAY, SOUTH OF MAIN AVE.
COUNTY OF SACRAMENTO, CA.
CARL PANATTONI - DEVELOPER

SITE PLAN

M Leo McGlade & associates, Inc.
1111 Agriculture Avenue, Sacramento, CA 95825 (916) 408-8300

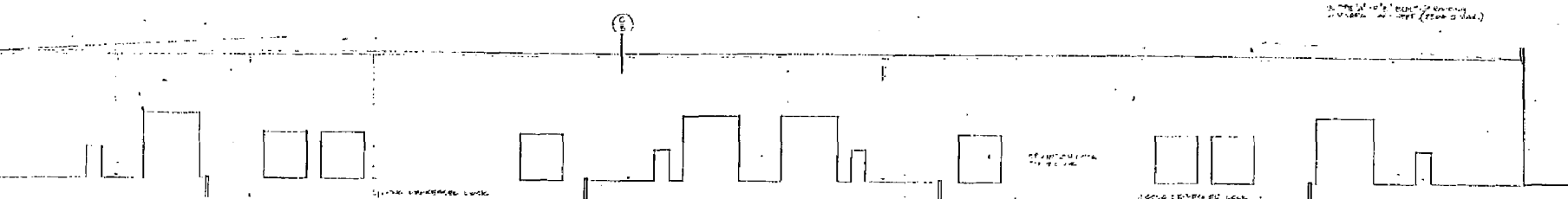
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D-87-292

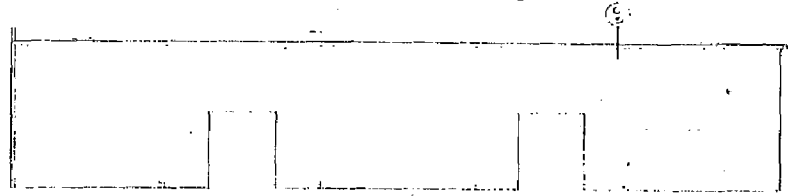
8-13-87

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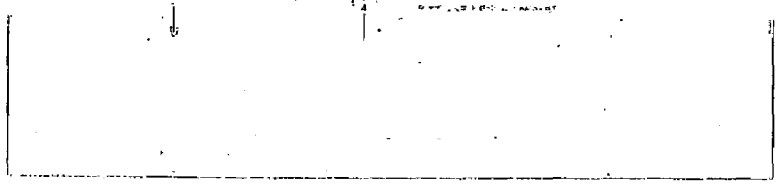
ELEVATIONS



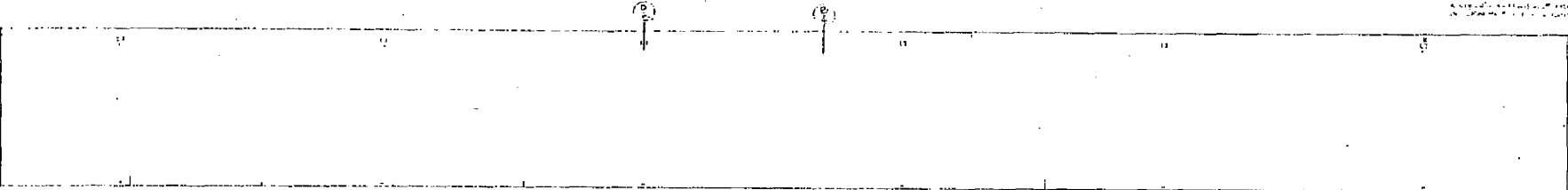
EAST ELEVATION



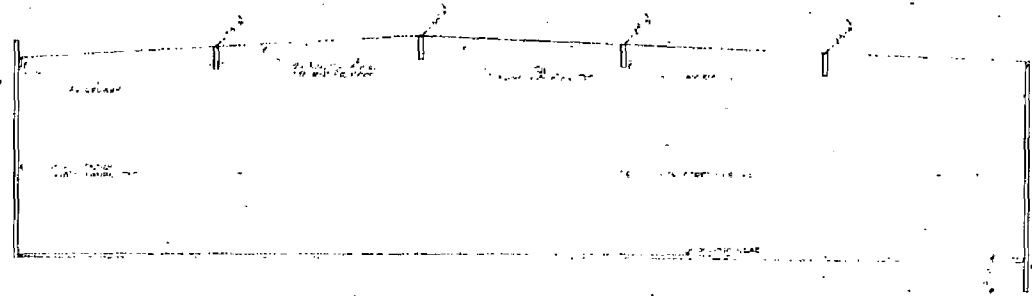
SOUTH ELEVATION



NORTH ELEVATION

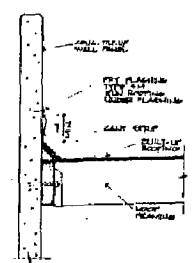


WEST ELEVATION

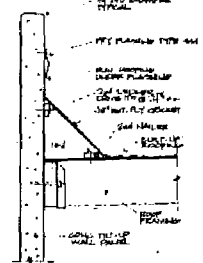


CROSS SECTION

A
4




SECTION



SECTION

REVISIONS	BY

Leo McGrade & Associates, Inc.


BUILDING - A

2- STORAGE BUILDINGS
 KELTON WAY, SOUTH OF MAIN AVE
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

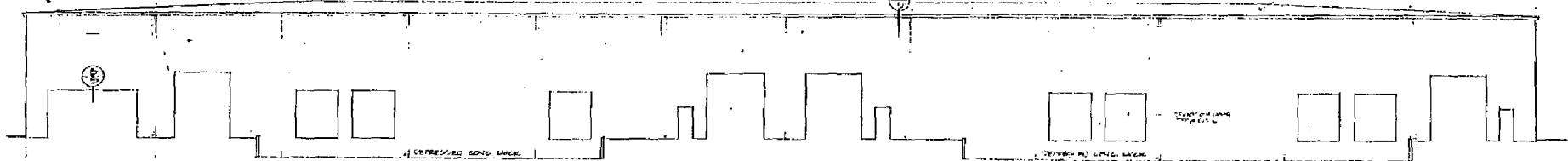
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Job	87-298
Sheet	4

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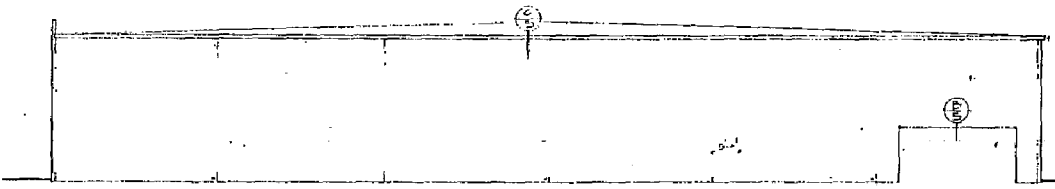
P-87-298

QUALITY WALL
FINISH



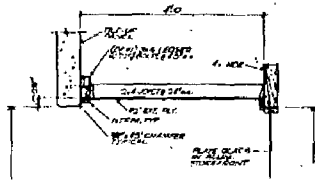
NORTH ELEVATION

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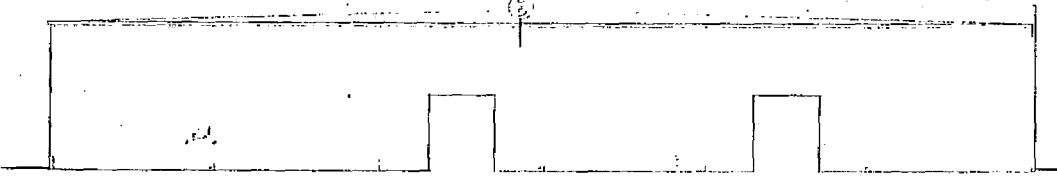
EAST ELEVATION

SCALE: 1/8" = 1'-0"



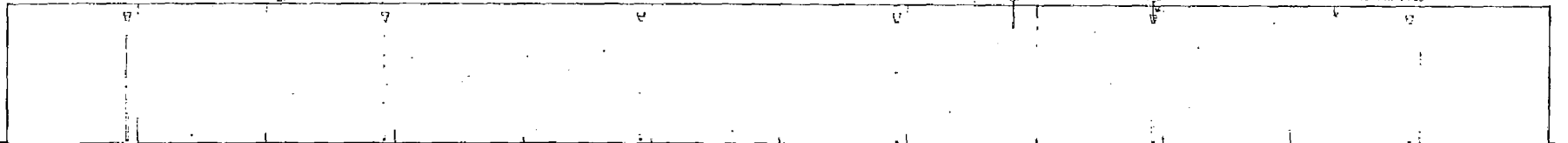
SECTION B-B

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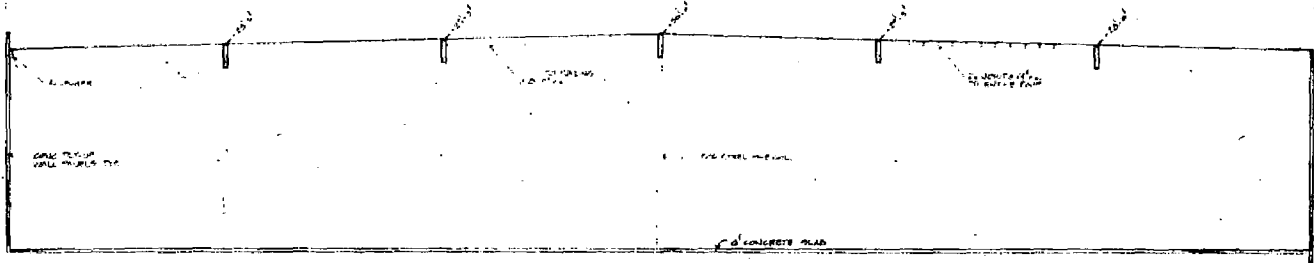
WEST ELEVATION

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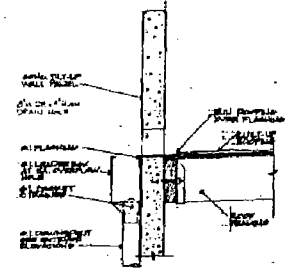
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SCALE: 1/8" = 1'-0"



CROSS SECTION A-A

SCALE: 1/4" = 1'-0"



SECTION C-C

SCALE: 1/4" = 1'-0"

REVISIONS	BY

Leo McGlade & Associates, Inc.
 1111 K STREET, SACRAMENTO, CALIF. 95811
 ARCHITECTS

BUILDING - B

2nd STORAGE BUILDINGS
 KELTON WAY, SOUTH OF MAIN AVE.
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

DATE: MAY 1987
 DRAWN BY: J. J. J.
 CHECKED BY: J. J. J.
 TITLE: 5

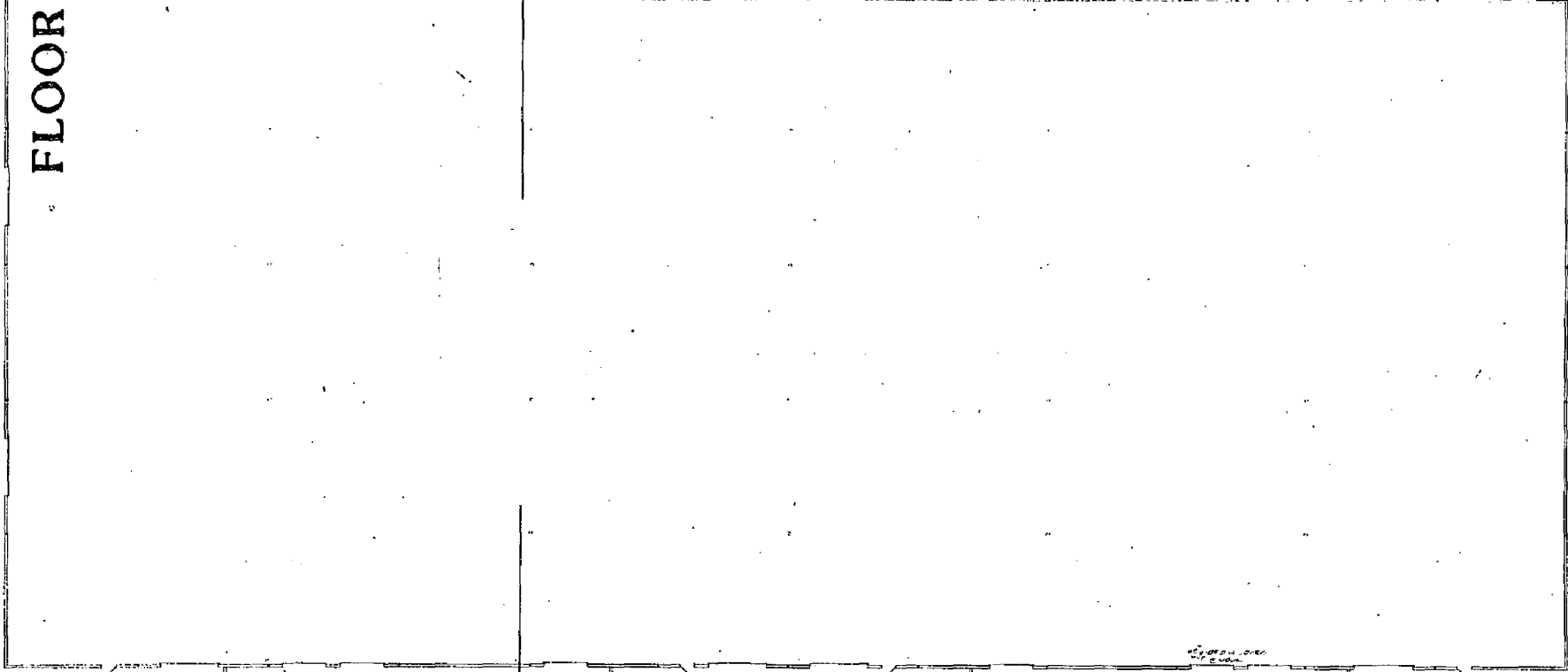
11

8-13-87

P-87-292

FLOOR PLAN

1/4" = 1'-0" (Scale)



FLOOR PLAN BUILDING - A

1. BUILDING SHALL DELIVER TO NOTE #10, SHEET-X
2. ENTIRE BUILDING SHALL BE SPRINKLERED, SEE NOTE #12, SHEET-X
3. "P" DESIGNATES "PRIMARY ENTRANCE", SEE NOTE #6, SHEET-X
4. ALL ROOMS SHALL COMPLY WITH NOTES #7 & #8, SHEET-X

Scale: 1/4" = 1'-0" →

NO.	DATE	BY	REVISIONS
1	MAR 14, 87	LEO MCGLOTH	ISSUED FOR PERMITS
2	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
3	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
4	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
5	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
6	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
7	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
8	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
9	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS
10	MAR 14, 87	LEO MCGLOTH	REVISED PER COMMENTS

2ND STORAGE BUILDINGS
 KELTON WAY, SOUTH OF MAIN AVE.
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

BUILDING - A

Leo McGloth & Associates, Inc.
 ARCHITECTS
 1000 J STREET, SUITE 100
 SACRAMENTO, CALIF. 95811
 (916) 441-1111

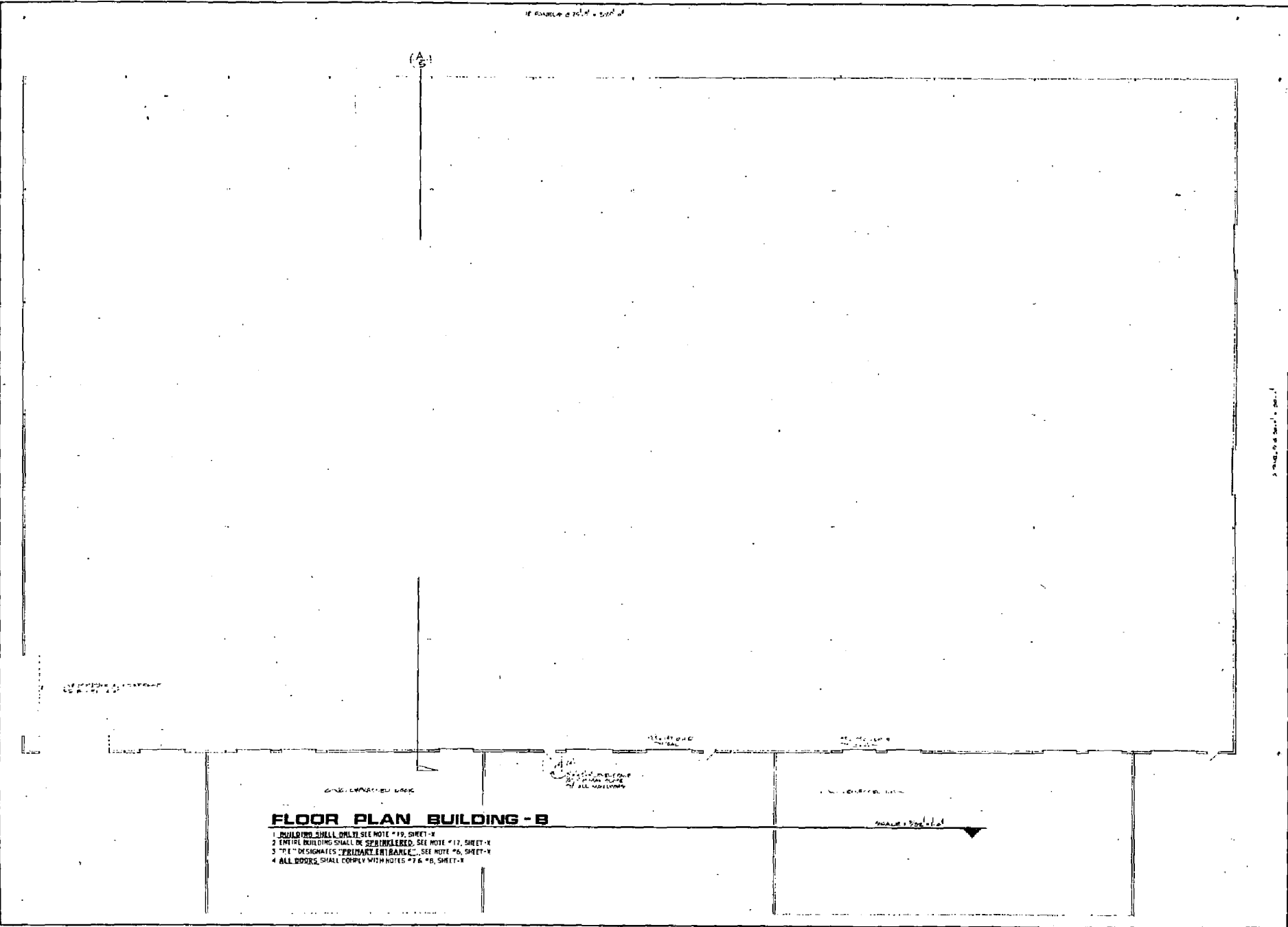
P-87-292

8-13-87

item 11

IF DIMENSIONS ARE NOT SHOWN

(5)



FLOOR PLAN BUILDING - B

- 1. BUILDING SHELL ONLY, SEE NOTE #19, SHEET-X
- 2. ENTIRE BUILDING SHALL BE SPRINKLERED, SEE NOTE #17, SHEET-X
- 3. "P" DESIGNATES "PRIMARY ENTRANCE", SEE NOTE #6, SHEET-X
- 4. ALL DOORS SHALL COMPLY WITH NOTES #7 & #8, SHEET-X

SCALE: 1/8" = 1'-0"

REVISIONS	BY



BUILDING - B

2ND STORAGE BUILDINGS
 KELTON WAY SOUTH OF MAIN AVE.
 COUNTY OF SACRAMENTO, CA.
 CARL PANATTONI - DEVELOPER

DATE	MAR 12, 87
SCALE	1/8" = 1'-0"
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BY	MSH
DATE	03-11-87
SHEET	33
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8-13-87

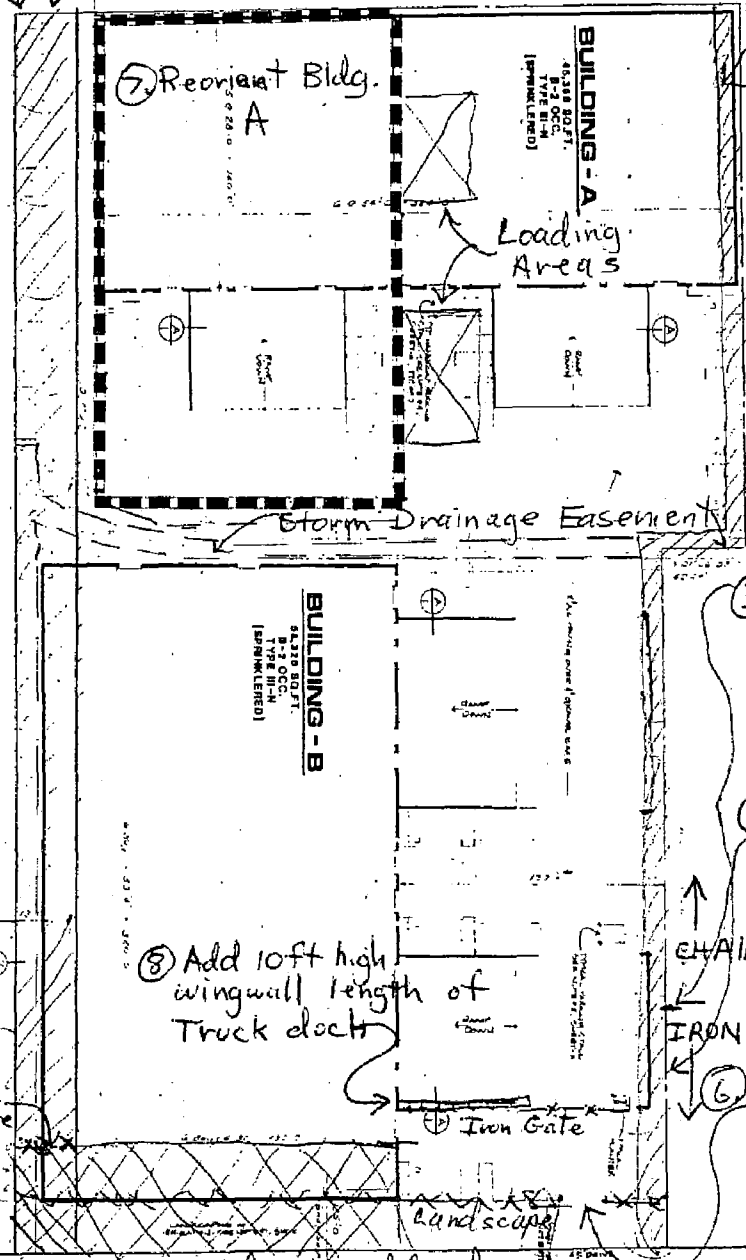
2-87-292

EXHIBIT A

STAFF MODIFIED SITE PLAN

- ① Add 6 ft high solid decorative masonry wall
- ② Add 25 ft wide landscape strip along South property line.

SITE PLAN
FOR HANDICAP PATH OF TRAVEL REQUIREMENTS, SEE NOTE #5, SHEET X



- ④ Add 10 ft. landscape planter strip along North property line - Reduce planter width to 6 ft. opposite truck loading bay's.

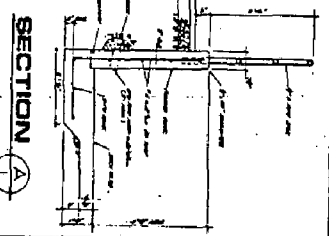
- ⑤ Add 6 ft high decorative wrought iron fence along North property line.

- ⑦ Construct 6 ft chainlink fence behind Haulin - Within setback

- ⑧ Add 10 ft high wingwall length of Truck dock

- ⑥ Add wrought-iron security gate - roll type & Fence along Kelton behind 25 ft setback or along wingwall

- ③ Increase Building Setback to 50 feet - Minimum of 25 feet landscaping from Kelton Way or reduce height of building to 12 ft @ 15 feet at 25 ft setback and increase to 29 ft. height at 50 ft. setback.



<p>DATE: 8-13-87 BY: [Signature] CHECKED: [Signature] SCALE: AS SHOWN</p>	<p>2- STORAGE BUILDINGS KELTON WAY, SOUTH OF MAIN AVE. COUNTY OF SACRAMENTO, CA CARL PANATTONI - DEVELOPER</p>	<p>SITE PLAN</p>	<p>M Leo McGlade & associates, Inc. 1417 Aster Way, Suite A Sacramento, CA 95825 (916) 484-2300</p>
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EXHIBIT B
CORRESPONDANCE
CITY PLANNING DIVISION

TO: Will Weitman, Senior Planner
Sacramento City Planning Department
1231 "I" Street, Suite 200
Sacramento, California 95814

AUG 5 1987

FROM: The Meadows Development Association
c/o Arek Firman
157 Gunnison Avenue
Sacramento, California 95838

RECEIVED

SUBJECT: P-87-292 -- Proposed Office and Warehouse Shell on
West side Kelton Way, 300 Feet South of Main Avenue

DATE: August 4, 1987

As a spokes person for the Meadows Development Association, I am submitting this letter to voice our increasing concerns at the efforts to further detract and negatively impact our residential neighborhood by the continued approvals made on commercial/industrial developments such as the proposed project to be located at the southwest corner of Kelton Way and Main Avenue in North Sacramento. It is our hope that, by making the Planning Commission aware of the issues most important to us, key actions will be taken by the Commission to minimize the potentially negative outgrowth of commercial development in our area.

We would like to see the following changes made in this proposal:

- 1) Reduce office warehouse buildings to four (4) 20,000 square feet units, with a maximum height of 20 feet and architecturally compatible with the residential community.
- 2) A 88 feet building setback off Kelton Way and a 25 feet from the southern property line (same building setback as Havlin Watkin).
- 3) Exposed aggregate surface with design variations.
- 4) On Kelton Way, frontage the use of a variety of surfaces (stone, brick, and wood) with windows, canopies, and awnings to establish architectural continuity with the residential neighborhood.
- 5) No access of Kelton Way. The street is for residential use only. All access is to be off Main Avenue. Additionally, the developer should improve the Main Avenue and Kelton Way intersection and incur all costs for sidewalks and curbs.

P-87-292

8-13-87

item //

Will Weitman, Senior Planner
Sacramento City Planning Department
RE: P-87-292
August 4, 1987
Page Two

- 6) Put monies into a Trust Fund for signal lights at the intersection of Kelton Way and Main Avenue.
- 7) Landscape a 25' wide strip with four (4) feet high beams and 15 gallon trees on 10 feet centers along the Kelton Way and southern property line. Also, buildings are to be surrounded by landscaping.
- 8) Restrict tenants to uses which do not produce noises or emissions outside the buildings (no woodworking, metal fabrication, or assembly shops).
- 9) All lighting to be directed on site and not to reflect off-site into the residential area.
- 10) Restrict hours of operations from sunrise to sunset or 7 a.m. to 8 p.m. for all tenants.
- 11) Require all loading and receiving docks and doors to face away from residential area.
- 12) Install a 6 feet high wrought iron fence along Kelton Way to the north property line to prevent unauthorized access.
- 13) Install a 8 feet high block wall to act as a sound barrier.
- 14) No public address systems outdoors.
- 15) Residencies requirement of 50% of the total employees for the entire site.
- 16) Require the developer to work with a community committee in establishing landscaping and architectural designs.

Will Weitman, Senior Planner
Sacramento City Planning Department
RE: P-87-292
August 4, 1987
Page Three

Sleazy development has been a negative aspect of poor planning in the past in Sacramento and we would like to prevent this activity from happening in our area. The points made in this letter have been presented in an attempt to blend the commercial/industrial development being brought to our residential neighborhood with the least intrusive effect. Our major concerns are that the construction design, size, and landscaping of this particular project exceed the inadequate standards of projects located along Main Avenue and Pell Drive, and that traffic control be implemented as requested and, as quickly as possible.

We thank the Planning Commission for their time and consideration of our presentation herein.

Respectfully submitted,

THE MEADOWS DEVELOPMENT ASSOCIATION

CITY PLANNING COMMISSION

1231 'I' STREET, SUITE 200, SACRAMENTO, CA 95814

Sarmat
only

APPLICANT	Murry Smith & Associates - 3020 Explorer Drive, Sacramento, CA 95827				
OWNER	Raybell c/o Chris Steele - 10969 Trade Center Dr. #100, Rancho Cordova 95670				
PLANS BY	Comstock Johnson Architects - 3212A Ramos Cir., Sacramento, CA 95827				
FILING DATE	9-20-85	ENVIR. DET.	10-14-85	REPORT BY	DH:sg
ASSESSOR'S-PCL. NO.	238-050-04; 238-060-01				

- APPLICATION:**
- A. Negative Declaration
 - B. Special Permit to locate five parking spaces on 0.2+ vacant acres located in the Single Family (R-1) zone
 - C. Plan Review of a 526,980+ square foot warehouse and office complex on 30.3+ vacant acres in the M-1(S)-R zone
 - D. Lot Line Adjustment to merge two parcels totaling 30.3+ acres in the M-1(S)-R and R-1 zones

LOCATION: Southeast corner of Raley Boulevard and Bell Avenue

PROPOSAL: The applicant is requesting the necessary entitlements to construct a two phase warehouse complex on 30 vacant acres for 526,980 square feet of warehouse.

PROJECT INFORMATION:

1974 General Plan Designation: Commercial and Offices
1984 North Sacramento Community Plan Designation: Labor Intensive (Office Commercial, Light Industrial)
Existing Zoning of Site: M-1(S)-R and R-1
Existing Land Use of Site: Vacant

Surrounding Land Use and Zoning:	Setbacks:	Provided	Required
North: Vacant; M-1(S)-R	Front:	85'	25'
South: Single family residential; R-1	Side(Int):	25'	15'
East: Mobile home park & PG&E power lines; R-1	Side(St):	25'	0
West: Tavern, vacant & single family; C-2-R & R-1	Rear:	25'	15'

Parking Required: Phase I - 209 spaces; Phase II - 525 spaces
Parking Provided: Phase I - 211 spaces; Phase II - 519 spaces
Property Dimensions: Irregular
Property Area: 30.3+ acres
Square Footage of Building: 526,980 square feet in 10 buildings;
Phase I - 145,259 square feet
Phase II - 381, 829 square feet
Height of Building: One story, 18 feet
Significant Features of Site: Vacant
Topography: Flat
Street Improvements: To be improved
Utilities: To be improved
Exterior Building Materials: Cream beige with dark store front
Exterior Building Materials: Concrete tilt-up with store front facade in glass panels
Estimated Employees: Phase I 236
Phase II 620 Total = 856

001701

APPLC. NO. P85-397 **MEETING DATE** October 24, 1985 **ITEM NO.** 25

2

PROJECT BACKGROUND: On November 20, 1978, the City Planning Division completed a preliminary review of a 124 unit single family subdivision for the 30 acre subject parcel. The Environmental Coordinator identified the exterior noise levels as being 10 to 15 decibels greater than the City's exterior noise standard of 60 dBA. The site was indicated by the Air Force's Air Installation Compatible Use Zone to have a noise level of Ldn 70-75 dBA. The request for a residential use was withdrawn by the applicant.

The proposed project site is designated for residential uses on the 1974 General Plan which was amended with the adoption of the 1984 North Sacramento Community Plan. The Community Plan indicates the site for Labor Intensive Office, Commercial and Light Industrial uses.

The subject site is presently zoned M-1(S)-R. The purpose of the "R" Review zone is to allow the Commission to review proposed projects in light of infrastructure deficiencies in the area, compatibility of adjacent existing land uses with the proposed project, and compatibility with the operations of McClellan A.F.B. operations. A mandatory Labor Intensive Overlay zoning was not applied to the site due to existing noise levels.

PROJECT PROPOSAL: The proposed project consists of Plan Review for two phase proposal; phase one covers approximately 10 acres and entails the construction of four warehouse type buildings containing a total of 145,000 square feet of lease area. Phase two covers the remaining 20 acres with approximately 381,828 square feet of warehouse in six buildings. Both phases provide sufficient parking to accommodate up to approximately 25% of the floor area for office use.

In addition to the plan review for the 526,980 square feet of industrial warehouse, the applicant is requesting a special permit to use a 0.2+ acre vacant parcel adjacent to the site in the Single Family Residential (R-1) zone for a portion of the parking and landscaping for the project. The applicant is also requesting a lot line adjustment to merge the 0.2+ acre parcel with the 30+ acre industrially zone parcel. The parcel is located near the northeast corner of MacArthur Street and Raley Boulevard.

PROJECT EVALUATION: Staff has the following comments regarding the proposed project:

- A. Land Use - The subject site is shown on the 1974 General Plan for Commercial and Office uses and the 1984 North Sacramento Community Plan and Labor Intensive (Office, Commercial, Light Industrial). Existing zoning of the site is Light Manufacturing with plan review and 25 foot landscape setback, M-1(S)-R. A mandatory Labor Intensive Overlay zoning was not applied to the site.

The site is vacant and bordered by a mixture of residential and commercial uses (see land use map). The 1984 North Sacramento Community Plan identified several major issues which directly affect the proposed project. They concern traffic, infrastructure and compatibility of future uses with surrounding land uses and the operations at McClellan A.F.B. The proposed project, as conditioned, will be consistent with the City General Plan and Community Plan.

B. Site Design

1. Parking

A total of 211 parking spaces are provided for phase I. The required parking is 209 spaces based upon a maximum of 25% office and 75% warehouse use. A special permit from the Planning Commission is necessary to increase the parking over the 25% for office use. Phase II parking provided is 519 spaces. (staff amended)

Estimated number of employees for phase I is 236 and phase II is 620 employees. Additional on-site parking may be necessary if an increase in employees occurs due to future tenants of the project site. Total employees is estimated at 856.

2. Setbacks

A 25 foot landscaped setback is required along Raley Boulevard and Bell Avenue street frontages. Building setbacks along east and south residentially zoned properties are a minimum of 15 feet. Staff recommends that a minimum of 50 feet setback for all buildings abutting residentially zoned property be established as shown on the site plan with a minimum of 25 feet. (staff amended)

3. Landscaping

Landscaping is proposed along Raley Boulevard and Bell Avenue. The berms shown on the applicant's plans indicate no relative height. Staff recommends that landscaping berms be undulating in height but no lower than four feet above sidewalk level. An eight foot wide landscape strip is proposed for the eastern and southern property lines. Also, an eight foot high solid masonry wall is proposed along this line. Staff recommends that split faced concrete block a decorative masonry or concrete wall be used on the eight foot high wall facing the residentially zoned areas subject to review by staff. (staff amended)

In reviewing the landscape plans, staff could not determine the appropriate areas used in the shade analysis. Staff recommends that when a revised plan showing modifications to the buildings and landscaping plans is prepared, the a shade analysis be presented to the Planning Director for phase I in sufficient detail to analyze the shading plan.

4. Building Design

Staff has the following comments regarding the design and layout of the proposed project. Staff recommends action only on phase I since elevations and floor plans for phase II were not provided for review and infrastructure is not available to serve Phase II at this time. (staff amended)

- a. A major issue identified by staff is the relative monotonous design of the exteriors of the concrete tilt up panels. The

applicant proposes to construct 10 concrete tilt up warehouse structures in two phases. The panels will be a cream colored beige with either a burgundy or green trim accent. The specific trim color has not been determined by the project architect. The office facade will have glass panels. The end facades facing Bell Avenue on Buildings A, B, C and D have glass panels between the concrete tilt up panels. The walls are uniform and offer no variation in depth. Staff recommends that the applicant redesign the front facade of Building A facing Raley Boulevard to provide modulation of the facade for variety in movement and depth along the street frontage. Staff also recommends that for phase I Buildings B and C have the Bell Avenue facades modulated so that movement and depth is afforded facing the street. The applicant proposes to install canopies over the office entryways and modify elevations to provide relief to the satisfaction of staff. (staff amended)

- b. A guard station is proposed off Raley Boulevard. A six foot chainlink fence gate in vinyl clad chainlink on wheels is proposed. Staff recommends that if fencing other than masonry block wall is proposed, including gates and fencing around the parking area for Building E, that a six foot decorative wrought iron fence be installed. The guard station should be architecturally compatible in color and design as the main structures.
- c. Based upon the prominence of the location, the west and north face of Building A should be redesigned to provide modulation and variation. The north facade of Buildings B and C with wing walls and trash enclosures should be redesigned to be totally integrated with the modulations preferred on all buildings facing a public street.
- d. Building setbacks as shown on the plans are adequate subject to the provision that no vehicular access be allowed from the rear of Building E. for all buildings from the adjacent property lines shall be 50 feet to allow for access and parking. (staff amended).
- e- Building E shows loading bays facing the residentially zoned properties to the south. City policy is to discourage loading docks when they face residential land uses since sounds related to loading and unloading disrupt the residential neighborhood. Building E is also poorly situated in that access to the south side is via a one-way lane, less than 25 feet in width. Staff recommends that the entire building be redesigned so that no loading bays face the R-1 zoned properties to the south and that adequate space be provided around the entire building for backing up trucks for loading. If automobile parking is proposed, then the minimum parking ordinance requirements shall apply. Buildings F and G in phase II will require redesign to relocate loading bays away from residential uses. (staff deleted).

Building E shall be designed as shown on submitted plans which does not have loading bays facing residentially zoned properties.

f. Exhibit L presents project design guidelines for the review and approval of subsequent building permits. Staff recommends that the Commission require the applicant to adhere to the Project Design Guidelines when revising the site plan to address all issues and conditions approved by the Commission for phase I.

C. Infrastructure - Under the "R" review, the policy of assuring adequate water, sewer, drainage and street improvements is required to be addressed. The Public Works Department has identified issues identified in exhibit M relative to adequacy of water supply, limited sewage capacity, street right-of-way, and limited drainage capacity. Public Works staff indicated a guarded approach to development of the site. Further discussion with Public Works resulted with a conclusion that a prudent approach would be to approve only phase I at this time. In order to properly serve phase II, major off site work will be required and unless the applicant is willing to carry the cost, phase II should not be approved. Work has begun on formation of an assessment district to implement the major improvements.

D. Transportation Management Plan - The 1984 North Sacramento Plan Final Environmental Impact Report includes as a traffic mitigation measure the following statements:

1. In the area north of I-880, all of the designated major streets should be upgraded as development proceeds in the future. Special studies should be undertaken at intersections to determine their ultimate lane configuration, signal requirements, and access control requirements. Potential funding mechanisms should be investigated, including the development of a benefit assessment district.
2. A number of new traffic signals will be required throughout the area as a result of the implementation of the Community Plan and development occurring in adjacent areas.
3. TSM actions should be implemented by the City wherever feasible to reduce auto usage and encourage use of alternative modes. These actions should include continued efforts to upgrade and maintain the bikeway system, support for ridesharing programs, and efforts to encourage mass transit usage.

Staff recommends that a transportation management plan be prepared for the project. As a component of the transportation management, that in-lieu measures be provided which may include the following items: bicycle locker and shower facilities; financial reimburse to Regional Transit to provide bus service to the site; development of a ride-sharing or vanpool service for all employees of the site; provision of a bus stop and/or shelter adjacent to the site; financial reimbursement to tenants who offer their employees opportunities to use modes of transportation other than the single occupancy automobile.

D. Comments From City Departments - The City Engineer, Traffic Engineer, Real Estate, Public Works - Water and Sewer, Regional Transit, and Design Review have reviewed the proposed project. Comments are previously included in relevant sections of the report. The City Traffic Engineer made the following comments:

1. A 45 foot wide driveway into a 26 foot wide gate is unacceptable. The truck gate should be separated from the automobile gate.
2. The automobile driveway on Bell Avenue next to the intersection is in a poor location.
3. Interior design is too tight for trucks in many places.
4. Street improvements are required.

Staff recommends that the revised site plan be prepared in consultation with the City Traffic Engineer to resolve differences as noted. The driveway into the parking lot for Building A near the intersection of Bell Avenue and Raley Boulevard fronting on Bell Avenue is not a automobile driveway but a turf blocked grass area for emergency access off Bell Avenue for the Fire Department.

The City Engineer has provided comments relative to water, sewage, drainage and roads which are presented in exhibit M and are listed below for phase I. Review of phase II improvements will occur at the time the Planning Commission reviews plans for phase II at a later date. The City Engineer noted that drainage is inadequate, sewer capacity is limited, and the present water system is inadequate for property surrounding the site.

The City Engineer recommended that the following requirements be placed upon the project for phase I:

1. Sewer and drain study and water supply study required.
2. Standard improvements are required, including off-site drain and sewer extensions on Raley Boulevard, and possibly including construction of dry sewer and drain lines on Bell Avenue. These dry sewer and drain lines will necessitate a drainage study providing ultimate design for the area. Phase I will be sewerd to the south to Sump No. 106.
3. Applicant would also be required to sign an agreement with the City to participate in the future assessment district proceedings to construct street, sewer, drainage, water, and street light improvements.
4. Soils testing for street design will be required.
5. Dedication of right-of-way along Raley Boulevard and Bell Avenue to a 45 foot half-section plus a five foot bike lane on Bell Avenue.
6. A water system supply study is required. Pipe size is adequate, reservoir capacity is not. The project may not meet fire flow requirements without additional improvements.

The City Fire Department has been contacted by the applicant relative to location of fire hydrants, line size and water pressure. The City Fire Marshal's office indicated to staff that all future tenants of the site will be required to comply with Fire Department requirements for storage of hazardous flammable products and the manufacturing of products at the site. Reserve water supply may be a problem for the site, which shall be resolved to the satisfaction of the City Fire Marshal's office.

CITY PLANNING COMMISSION

915 "I" STREET - SACRAMENTO, CALIFORNIA 95814

APPROVED

Date 11-21-75

APPLICANT	Williams & Associates-2426 Glendale Dr., Sacramento, California		
OWNER	Havlin Witkin-4351 Jetway Court, North Highlands, California 95660		
PLANS BY	Williams & Associates-2426 Glendale Dr., Sacramento, California		
FILING DATE	11-14-74	50 DAY CPC ACTION DATE	4-30-75
		REPORT BY:	REH
NEGATIVE DEC.	11-27-74	EIR	--
		ASSESSOR'S PCL. NO.	237-020-06

APPROVED

APPLICATION: Tentative Subdivision Map Lot 27, Rio Linda Subdivision Unit 8. (P-6350).

CITY PLANNING COMMISSION

LOCATION: Southwest corner of the intersection of Main Avenue and Kelton Way. DATE 3-25-75

This is a proposal to divide 10.5+ acres into three parcels for the purpose of industrial development. The site is currently undeveloped and is zoned "M-1-SR". Parcel 1 will be developed as a warehouse. No specific development plans are available for parcels 2 and 3 at this time.

Surrounding properties include agricultural uses to the north and east, residentially subdivided land that is vacant to the south, and commercial and industrial properties to the west. Surrounding properties to the north and east are zoned "A" Agricultural; to the south is "R-1" single family residential zone; to the west is "M-1" light industrial zone.

This item was continued indefinitely by the Commission at the December 10, 1974 meeting as the proposed tentative map was not in conformance with the North Norwood Community Plan. The applicant has requested an amendment to said Community Plan (P-6335), which was favorably acted upon by the Commission at the February 25, 1975 meeting.

On March 11, 1975 the Planning Commission affirmed the decision of the Environmental Coordinator and determined that this proposed land division would not have a significant environmental impact. The following findings are required pursuant to Section 66474 of the Government Code:

- a. The map and the design and improvement of the proposed subdivision are consistent with applicable general and specific plans.
- b. The site is physically suitable for the proposed density and type of development.
- c. The design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish, or wildlife, or their habitat.
- d. The design of the subdivision or the type of improvements is not likely to cause serious public health problems.
- e. The design of the subdivision or the type of improvements will not conflict with easements acquired by the public at large, or access through, or use of property within the proposed subdivision.

Should the Commission favorably recommend the map to the City Council, it is suggested that said recommendation be made subject to a request that the following conditions be imposed upon the map's approval by the Council if such approval be given:

1. Secure the written approval of the City Fire Marshal as to the manner in which the subdivider will provide fire hydrants within the subdivisions at those locations designated by the City Fire Marshal.
2. Secure the written approval of the Water and Sewer Division of the City as to the manner in which a public water supply and sanitary sewer facilities will be provided to proposed parcel 3.
3. Comply fully with the provisions of Article 8 and Paragraph C of Section 40.811 of the subdivision regulations regarding subdivision improvements. Any such improvements shall be accomplished to the satisfaction of the City Engineer.
4. Secure written approval of the Real Estate Division that there are no delinquent taxes and that current taxes are paid.
5. Comply with all applicable City ordinances and codes.

CITY PLANNING COMMISSION

915 "I" STREET - SACRAMENTO, CALIFORNIA 95814

APPLICANT	Williams & Associates-2426 Glendale Dr., Sacramento, California		
OWNER	Havlin Witkin-4351 Jetway Court, North Highlands, California 95660		
PLANS BY	Williams & Associates-2426 Glendale Dr., Sacramento, California		
FILING DATE	11-14-74	50 DAY CPC ACTION DATE	4-30-75
		REPORT BY:	REH
NEGATIVE DEC.	11-27-74	EIR	--
		ASSESSOR'S PCL. NO.	237-020-06

APPLICATION: Tentative Subdivision Map Lot 27, Rio Linda Subdivision Unit 8. (P-6350).

LOCATION: Southwest corner of the intersection of Main Avenue and Kelton Way.

This is a proposal to divide 10.5± acres into three parcels for the purpose of industrial development. The site is currently undeveloped and is zoned "M-1-SR". Parcel 1 will be developed as a warehouse. No specific development plans are available for parcels 2 and 3 at this time.

Surrounding properties include agricultural uses to the north and east, residentially subdivided land that is vacant to the south, and commercial and industrial properties to the west. Surrounding properties to the north and east are zoned "A" Agricultural; to the south is "R-1" single family residential zone; to the west is "M-1" light industrial zone.

This item was continued indefinitely by the Commission at the December 10, 1974 meeting as the proposed tentative map was not in conformance with the North Norwood Community Plan. The applicant has requested an amendment to said Community Plan (P-6335), which was favorably acted upon by the Commission at the February 25, 1975 meeting.

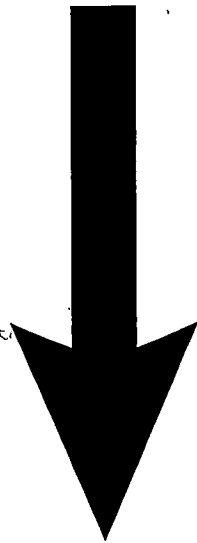
On March 11, 1975 the Planning Commission affirmed the decision of the Environmental Coordinator and determined that this proposed land division would not have a significant environmental impact. The following findings are required pursuant to Section 66474 of the Government Code:

- a. The map and the design and improvement of the proposed subdivision are consistent with applicable general and specific plans.
- b. The site is physically suitable for the proposed density and type of development.
- c. The design of the subdivision or the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish, or wildlife, or their habitat.
- d. The design of the subdivision or the type of improvements is not likely to cause serious public health problems.
- e. The design of the subdivision or the type of improvements will not conflict with easements acquired by the public at large, or access through, or use of property within the proposed subdivision.

Should the Commission favorably recommend the map to the City Council, it is suggested that said recommendation be made subject to a request that the following conditions be imposed upon the map's approval by the Council if such approval be given:

1. Secure the written approval of the City Fire Marshal as to the manner in which the subdivider will provide fire hydrants within the subdivisions at those locations designated by the City Fire Marshal.
2. Secure the written approval of the Water and Sewer Division of the City as to the manner in which a public water supply and sanitary sewer facilities will be provided to proposed parcel 3.
3. Comply fully with the provisions of Article 8 and Paragraph C of Section 40.811 of the subdivision regulations regarding subdivision improvements. Any such improvements shall be accomplished to the satisfaction of the City Engineer.
4. Secure written approval of the Real Estate Division that there are no delinquent taxes and that current taxes are paid.
5. Comply with all applicable City ordinances and codes.

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CITY PLANNING COMMISSION

1231 'I' STREET, SUITE 200, SACRAMENTO, CA 95814

APPLICANT	SPINK CORPORATION, P O BOX 2511, Sacramento, California 95811
OWNER	RIVERVIEW RANCH PARTANERSHIP, 7700 College Town Drive, Sac., CA 95826
PLANS BY	SPINK CORPORATION, P O BOX 2511, Sacramento, California 95811
FILING DATE	6/19/87
ENVIR. DET.	7/13/87
Nec. Dec.	
REPORT BY	SD/vf
ASSESSOR'S-PCL. NO.	274-0030-066

- APPLICATION:
- A. Negative Declaration
 - B. Amend Riverview Ranch PUD Schematic Plan from 144 condominium units and 109 townhouses to 101 petite units and six standard single family units.
 - C. Rezone 7.8+ vacant acres from Multi-Family (R-2B) to Single Family Alternative (R-1A).
 - D. Tentative Map (P87-286)
 - E. Special Permit to construct 101 single family petite units in the R-1A zone.
 - F. Subdivision Modification to waive standard street improvements.

LOCATION: Northeast corner of Orchard Lane and Garden Highway.

PROPOSAL: The applicant is requesting the necessary entitlements to develop 101 single family petites and six standard single units to be known as Brisas Del Rio in the Riverview Ranch PUD

PROJECT INFORMATION:

1974 General Plan Designation as amended:	Low Density residential
Interim South Natomas Community Plan Designation:	Residential; 11-21 du/ac
Existing Zoning of Site:	R-2B, R-1A
Existing Land Use of Site:	Vacant

Surrounding Land Use and Zoning:	Setbacks:	Required	Provided
North: Vacant; R-3 PUD	Front:	Determined	12.5' Min.
South: Residential & Commercial Office; F	Side(Int):	By	5' Min.
East : Vacant; OB-PUD	Side(St):	Planning	12.5' Min.
West : Condominiums; R-1-A	Rear:	Commission	12.5' Min.

Property Dimensions:	Irregular
Property Area:	17+ acres
Density of Development:	8.5 d. u. per acre

001206

Topography:	Flat
Street Improvements:	To be provided
Utilities:	To be provided
Exterior Building Materials:	Stucco and wood siding
Roof Material:	Tile

<u>Size of Units:</u>	<u>Plan 1</u>	<u>Plan 2</u>	<u>Plan 3</u>
	1,370 sq. ft.	1,780 sq. ft.	1,947 sq. ft.
	2 bdr/2 bath	3 bdr/3 bath	3 bed/2-1/2 bath
	1 story	2 story	2 story
	2 car garage	2 car garage	2 car garage

SUBDIVISION REVIEW COMMITTEE RECOMMENDATION: On July 8, 1987, by a vote of five ayes, four absent, the Subdivision Review Committee voted to recommend approval of the Tentative Map and Subdivision Modification subject to conditions which are attached.

BACKGROUND INFORMATION: On April 9, 1985, the City Council granted an appeal of the Planning Commission's denial of the rezone and special permit requests necessary to develop the subject site with 144 condominium units and 109 townhouse units (P85-063). On May 13, 1986, the Council approved the necessary entitlements to cause zoning boundaries to match the alignment of River Plaza Drive (P86-105) (See Exhibit A).

PROJECT EVALUATION: Staff has made the following findings:

A. Land Use

The subject site is designated for residential uses in the 1974 General Plan. The site is designated for 11 to 21 units per acre in the Interim South Natomas Community Plan. The site currently designated for townhouses and condominiums in the River View Ranch PUD Schematic Plan. The site is surrounded by the following uses:

residence and drainage canal to the east; residential and commercial to the south, residential townhouses to the west; and vacant land designated for apartments to the north. The property to the north is a portion of the River View Ranch PUD.

B. Rezoning and Schematic Plan Amendment

The northwest portion of the site is currently zoned Multi-Family (R-2B) and designated for 144 condominium units (See Exhibit A). The remainder of the site is zoned Single Family Alternative (R-1A) and designated for 109 townhouses. The applicant proposes to rezone the 7.8+ acres from R-2B to R-

1A and redesignate the entire site in order to develop 101 petite units and six standard single family units. The proposed project achieves a net density of 8.5 units per acre. Staff notes that this proposal is consistent with the original land use designation of the South Natomas Community Plan which was amended in 1985 at the developer's assurance that the site was unsuitable for single family development.

Staff has no objection to the requested rezoning and PUD Schematic Plan Amendment. The density of the overall PUD is 12.1 units per net acre which is consistent with the Community Plan. The applicant proposes two different housing types which is also consistent with the Community Plan. In addition, the proposed use is compatible with surrounding residential uses.

C. Design

The intent of petite housing is to offer affordable single family ownership opportunities by increasing the density slightly without appearing to squeeze detached single family structures together. In the past, this has resulted in long narrow lots, generally 40 to 45 feet wide by 100 feet deep, with 4,000+ sq. ft. of area. The applicant proposes a new approach to petite housing by creating lots typically 50 ft. wide by 80 ft. deep. Staff supports this new concept in that it permits affordable single family ownership opportunities, greater design flexibility along the street frontages and the lots are a minimum of 4,000 sq. ft. which has been found to be a workable area in the past.

The applicant proposes a series of three cul-de-sacs along River Plaza Drive. There will be a slope down from the drain canal crossing. Public Works, therefore, requests that the main entrance from River Plaza Drive be relocated from B Street to F Court. Staff supports this design because it does not necessitate a long unbroken wall along River Plaza Drive.

The applicant proposes six lots which back up to Orchard Lane. These are to be standard single family sized lots (52' x 100+') in order to meet Community Plan requirements of three housing types in projects of 30+ acres. There is a row of walnut trees on Orchard Lane adjacent to these lots. Staff has requested that efforts be made to retain these trees to buffer these lots from vehicles passing on Orchard Lane and looking down into these lots.

Garden Highway is on the river levee and there is not sufficient room for standard street improvements. The Subdivision Review Committee, therefore, recommends that curbs, gutters and sidewalks be waived along the Garden Highway frontage.

D. Special Permit

The applicant proposed three floor plans. The units range from 1,370 sq. ft. to 1,947 sq. ft. in size. Plan 1, 1,370 sq. ft., has two bedrooms, two bathes, a two-car garage and a fireplace. This is a one-story model. Plan two, 1,780 sq. ft., has three bedrooms, three bathes, a two-car garage and a fireplace. This is a two-story model. Plan three, 1,947 sq. ft. has three bedrooms, two and one-half bathes, a two-car garage and two fireplaces. Exterior building materials consist of stucco and wood siding with tile roofing. Exterior colors will be earth tones. Each floor plan has three elevations.

The wider lots offer greater design flexibility so that the streetscape is not dominated by garage doors. On Plan One, the garage is set in back of a bedroom bay window. These wider lots offer more opportunity to view the street from the interior living area. The outdoor living area is in the back of each unit with a logical relationship to indoor living area.

Each unit has a minimum five foot side yard setback. Driveways are 20 feet in depth. Front building setbacks are primarily 20 feet. Plan One may have a 12-1/2 foot setback for the bedroom bay window. The applicant will attempt to situate these units on longer lots whenever possible to achieve greater front setback. Rear setbacks will be generally 15 feet or more, however, kitchen nook bay windows may go down to 12-1/2 feet. Since these units do not back up to existing residential uses, staff has no objection to this proposal. Staff cautions that the 40 percent lot coverage requirement is not variable under the R-1A zone and must be met.

The Police Department recommends that the cul-de-sacs along River Plaza Drive be closed off to inhibit criminal access into the subdivision. Staff recommends that the yards along River Plaza Drive be enclosed with a decorative masonry wall ~~with 10 foot openings~~ *to the front setback area openings* at each cul-de-sac enclosed with decorative wrought ^{with} iron gates. Wall design and materials shall be approved by the Planning Director prior to issuance of building permits. The wall shall be placed ~~12-1/2~~ *five* feet from the property line. Staff recommends the front and street side yards, including those along River Plaza Drive, be landscaped and irrigated by the applicant.

E. Parkland Dedication

The Planning and Community Services Divisions have determined that Parkland Dedication in-lieu fees are appropriate. Fees will be based upon 1.984 acres of land multiplied by the per acre value established by the applicant's appraiser.

ENVIRONMENTAL DETERMINATION: The Environmental Coordinator has determined that the project will not have a significant impact on the environment and has filed a Negative Declaration with the following mitigation measures regarding trees and cultural deposits:

1. Plant Life

The site contains over 50 trees and a small orchard. The applicant is proposing to remove the trees bordering Orchard Lane, remove the orchard and remove several trees in a proposed right-of-way paralleling Garden Highway. While none of the trees' diameter at breast height exceeds 100 inches, almost all of the trees are healthy, mature and vital. To reduce the environmental impact of the tree removal to a less than significant level, the applicant must comply with the following:

a. The applicant shall submit a tree removal plan for the review and approval of the City Arborist. No trees shall be removed without first acquiring this approval. The applicant shall protect and retain those trees identified by the City Arborist for preservation by ensuring the following be complied with to the satisfaction of the City Planning Director and City Arborist prior to the issuance of building permit:

1. No action shall be taken which will harm the health, vitality or longevity of the trees to be preserved;
2. The applicant shall comply with any measures deemed necessary to preserve the trees after review and approval of the tree survey. These include, but are not limited to:
 - grading, trenching, cutting and/or filling within the driplines of the trees shall not occur.
 - a temporary six foot high chain link fence shall be placed around the driplines of the trees during construction to prevent soil compaction resulting from stacked construction materials, equipment and vehicular traffic. These fences shall be erected prior to issuance of a building permit and shall remain installed until landscaping commences.
 - irrigation within the driplines shall be prohibited for the oak trees to be retained.
 - roadway and building foundations shall not extend into any driplines.

2. Cultural Resources

The project is located in an area of potential archaeological sensitivity, consequently, the applicant shall comply with the following:

- If unusual amounts of bone, shell, or artifacts are uncovered, work within 50 meters of the area will stop immediately and a qualified archaeologist will be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impacts to a less than significant level, before construction continues.

RECOMMENDATION: Staff recommends the following actions:

1. Ratify the Negative Declaration.
2. Recommend approval of the Riverview Ranch PUD Schematic Plan from 144 condominium units and 109 townhouse units to 101 petite units and six standard single family units.
3. Recommend approval of the Rezoning of 7.8+ acres from Multi-Family (R-2B) to Single Family Alternative (R-1A).
4. Recommend approval of the Tentative Map subject to conditions.
5. Approve the Special Permit subject to conditions and based upon findings of fact which follow.
6. Recommend approval of the Subdivision Modification to waive curbs, gutters and sidewalks on Garden Highway.

Conditions - Tentative Map

The applicant shall satisfy each of the following conditions prior to filing the final map unless a different time for compliance is specifically noted:

1. Provide standard subdivision improvements and street lights on Garden Highway pursuant to Section 40.811 of the City Code.
2. Prepare a sewer and drainage study for the review and approval of the City Engineer.
3. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments.

4. Pursuant to City Code Section 40.1302 (Parkland Dedication), the applicant shall submit to the City an appraisal of the property to be subdivided and pay the required Parkland Dedication in-lieu fees. The appraisal shall be dated not more than 90 days prior to the filing of the final map (1.1984 ac.).
5. Pursuant to City Code Section 40.319-1, the applicant shall indicate easements on the final map to allow for the placement of centralized mail delivery units. The specific locations for such easements shall be subject to review and approval of the City Engineer after consultation with the United States Postal Service.
6. The applicant/developer shall designate and place on the final map, those structures and/or lots which will meet the required 80 percent south orientation (including solar access) to the satisfaction of the Planning Director, or comply with Title 24 requirements of the Uniform Building Code.
7. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impact to a less than significant effect before construction resumes. A note shall be placed on the final improvement plans referencing this condition.
8. Meet all County Sanitation District requirements.
9. Submit a soils test prepared by a registered engineer to be used in street design.
10. Widen pavement along Garden Highway per study on file with the City.
11. Submit a seepage study prepared by a registered engineer which identifies and recommends solutions for ground water related problems which may occur in both the subdivision lots and the public right-of-way; appropriate facilities shall be constructed to alleviate those problems.
12. Street sections shall be designed to provide for stabilized subgrades and pavement under high ground water conditions.
13. Dedicate a standard 12.5 foot Public Utility Easement for underground electrical facilities and appurtenances adjacent to all public ways.
14. Dedicate Lot A as a PUE for overhead and underground electrical facilities and appurtenances.
15. Show the 10 foot overhead electrical facility easement recorded in Book 412 of Deeds, Page 491 located adjacent to the levee within Lot A.

16. Contact RD1000 and pay fees, if any, resulting from increased run-off.
17. Remove all cement and metal piping to an approved dump site to the satisfaction of the City Planning Director and City/County Health Department.
18. Relocate access point from "B" Street to "F" Court and provide "F" Court a 50 ft. right-of-way to A Street.
19. Dedicate A Street to a 50 ft. right-of-way from Orchard Lane to B Street.
20. A portion of the property lies in a Federal Flood Hazard Area. No development can be approved until appropriate measures are taken to remove the area from the flood zone. Applicant shall comply with all F.E.M.A regulations.
21. Dedicate right-of-way along River Plaza Drive and Orchard Lane for slope easements.
22. Show all existing easements.
23. Minimum lot grade shall be one foot above the 100 year flood plan elevation.
24. Dedicate Lot A to the City.
25. Dedicate a 10 ft. levee and maintenance easement at the toe of the levee.
26. Work with the Public Works Division to retain trees in the Orchard Lane right-of-way if possible.
27. The applicant shall submit a tree removal plan for review and approval of the City Arborist. The applicant shall protect and retain those trees identified by the City Arborist. No trees shall be removed without the prior approval of the City Arborist.

Conditions - Special Permit

1. Floor plans and elevations are approved as submitted. Project shall be constructed per the submitted plans.
2. A master plan indicating building setbacks shall be submitted and approved by staff prior to issuance of building permits.

- openings. at the
and
location
3. A decorative masonry wall with wrought iron gates at cul-de-sacs shall be constructed on River Plaza Drive. The decorative wall shall also be installed along Orchard Lane. Design and building materials shall be approved by staff prior to issuance of building permits.
 4. Landscaping and irrigation plans shall be reviewed and approved by the City Landscape Architect prior to issuance of building permits. Landscaping and irrigation shall be installed by the applicant.

Findings of Fact - Special Permit

1. The project, as conditioned, is based upon sound principles of land use in that:
 - a. it allows affordable single family ownership;
 - b. lot sizes are smaller than standard without appearing to squeeze units together;
 - c. it inhibits access of criminal elements;
 - d. lots are a minimum of 4,000 sq. ft.
2. The project will not be detrimental to public health, safety or welfare or result in a nuisance in that:
 - a. the project does not back up to existing single family residential so that rear yard setbacks less than 15 feet will be detrimental;
 - b. the streetscape is not dominated by garage fronts;
 - c. adequate view of the street is provided for security purposes.
3. The proposed project is consistent with the City's Discretionary Interim Land Use Policy in that the site is designated for residential uses, 11-21 du/ac., in the Interim South Natomas Community Plan and the proposed project, together with the remainder of Riverview Ranch PUD conforms with the plan designation.

3. A decorative masonry wall with wrought iron gates at end-gates shall be constructed on River Plaza Drive. The decorative wall shall also be installed along Orchard Lane. Design and building materials shall be approved by staff prior to issuance of building permits.

4. Landscaping and irrigation plans shall be reviewed and approved by the City Landscape Architect prior to issuance of building permits. Landscaping and irrigation shall be installed by the applicant.

Findings of Fact - Special Permit

1. The project, as conditioned, is based upon sound principles of land use in that:

- a. it allows affordable single family ownership;
- b. lot sizes are smaller than standards without appearing to squeeze units together;
- c. it prohibits access of criminal elements;
- d. lots are a minimum of 0.030 sq. ft.

2. The project will not be detrimental to public health, safety or welfare or result in a nuisance in that:

- a. the project does not back up to existing single family residential so that rear yard setbacks less than 15 feet will be detrimental;
- b. the streetscape is not dominated by garage fronts;
- c. adequate view of the street is provided for security purposes.

3. The proposed project is consistent with the City's discretionary Interim Land Use Policy in that the site is designated for residential use, 11-21 (a)(c). In the Interim South Natomas Community Plan and the proposed project, together with the remainder of Riverview Ranch PUD conforms with the plan designation.

Attachment A
Initial Study Discussion
Brisas Del Rio (P87-286)

Proposed Project

The applicant is proposing to rezone 17.0± acres located at the northeast corner of Garden Highway and Orchard Lane from R-2B to R-1A and subdivide the site into 102 single family "petite lots, five standard single family lots one levee parkway lot and one remainder lot northwest of the 107 lots. The site currently contains a small orchard, numerous mature oak and walnut trees and a temporary construction yard for an apartment complex north of the subject site. The project is consistent with the General Plan which designates the site medium density residential.

4. Plant Life

The site contains over 50 trees and a small orchard. The applicant is proposing to remove the trees bordering Orchard Lane, remove the orchard and remove several trees in a proposed right-of-way paralleling Garden Highway. While none of the trees' diameter at breast height exceeds 100 inches, almost all of the trees are healthy, mature and vital. To reduce the environmental impact of the tree removal to a less than significant level, the applicant must comply with the following.

The following shall submit a tree removal plan for the review and approval of the City Arborist. No trees shall be removed without first acquiring this approval. The applicant shall protect and retain those trees identified by the City Arborist for preservation by ensuring the following be complied with to the satisfaction of the City Planning Director and City Arborist prior to the issue of a building permit.

1. No action shall be taken which will harm the health, vitality or longevity of the trees to be preserved.
2. The applicant shall comply with any measures deemed necessary to preserve the trees after review and approval of the tree survey. These include but are not limited to:
 - grading, trenching, cutting and/or filling within the driplines of the trees shall not occur.
 - a temporary six foot high chain link fence shall be placed around the driplines of the trees during construction to prevent soil compaction resulting from stacked construction materials, equipment and vehicular traffic. These fences shall be erected prior to issuance of a building permit and shall remain installed until landscaping commences.

001380

- irrigation within the driplines shall be prohibited for the oaks to be retained.
- roadway and building foundations shall not extend into any driplines.

20. Cultural Resources

The project is located in an area of potential archaeological sensitivity, consequently, the applicant shall comply with the following:

- If unusual amounts of bone, shell, or artifacts are uncovered, work within 50 meters of the area will stop immediately and a qualified archaeologist will be consulted to develop if necessary, further mitigation measures to reduce any archaeological impacts to a less than significant level, before construction continues.

001397

Attachment A
Structural Study Discussion
Encinas Del Rio (PR7-286)

Proposed Project

located at the northeast corner of Garden Highway and Orchard
The applicant is proposing to remove 17 of approximately 19
from R2B to R1A and subdivide the site into
102 single-family lots, 5 detached lots, 5 standard
single-family lots, 5 large detached lots and
one medium-density lot north of the 0107 lots.
The site currently contains a single
orchard, numerous mature oak, and oaknut
trees and a temporary construction yard for
an apartment complex with the proposed floor
The project is consistent with the General Plan
which designates the site medium density
residential

4 Plant Life

The site contains over 50 trees and a single
orchard. The applicant is proposing to
remove the trees bordering Orchard Lane
remove the orchard and remove several trees
in a proposed right-of-way paralleling
Garden Highway (which runs along the area)
diameter of breast height exceeds 100 inches.
Almost all of the trees are medium, mature and vital.
To reduce the environmental impact of the
tree removal to a level that is significant level
the applicant must comply with the following:

The applicant shall submit a tree-removal
plan for the review and approval of the
City Arborist. No tree shall be removed
without first securing tree approval. The
applicant shall provide and retain these
tree identification by the City Arborist for
preservation by ensuring the following
be complied with to the satisfaction

of the City Planning Director and City Arborist prior to the issue of a building permit

1. No action shall be taken which will harm the health, vitality or longevity of the trees to be preserved.

2. The applicant shall comply with any measures deemed necessary to preserve the trees after review and approval of the tree survey. These include but are not limited to:

- grading, trenching, cutting and/or filling within the driplines of the trees shall not occur.
- a temporary 6 foot high Chain Link fence shall be placed around the driplines of the trees during construction to prevent soil compaction resulting from stacked construction materials and equipment and vehicular traffic. These fences shall be erected prior to issuance of a building permit and shall remain installed until landscaping commences.
- irrigation within the driplines shall be prohibited for the cars to be retained.
- roadways and building foundations shall not extend into any driplines.

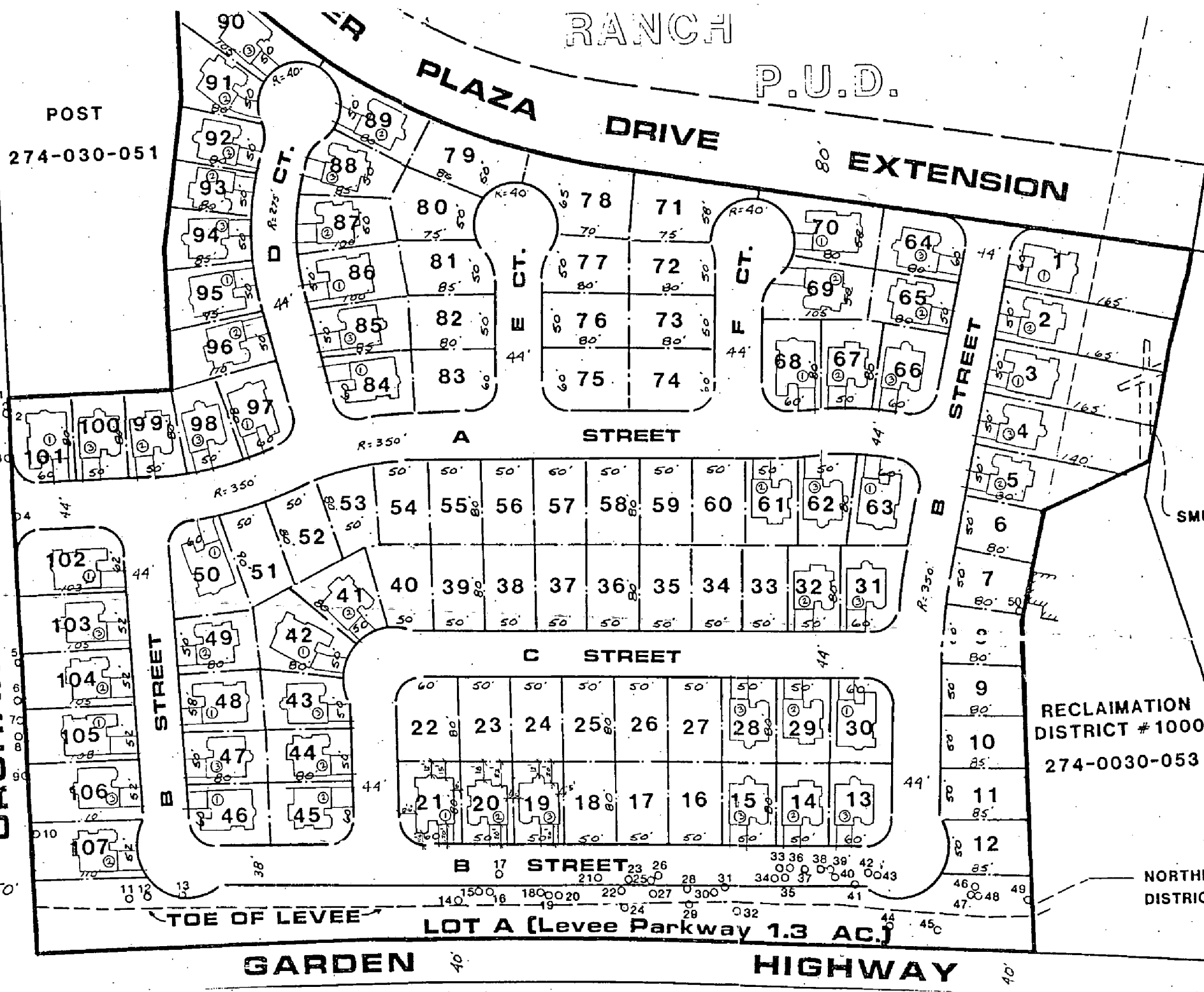
20. Cultural Resources

The project is located in an area of potential archaeological sensitivity, consequently, the applicant shall comply with the following:

- if unusual amounts of bone, shell, or artifacts are uncovered, work within 50 meters of the area will stop immediately and a qualified archaeologist will be consulted to develop if necessary, further mitigation measures to reduce any archaeological impacts to a less than significant level, before construction continues.

001399

TYPICAL SCHEMATIC PLAN
 SWALLOW'S NEST
 ORCHARD LANE



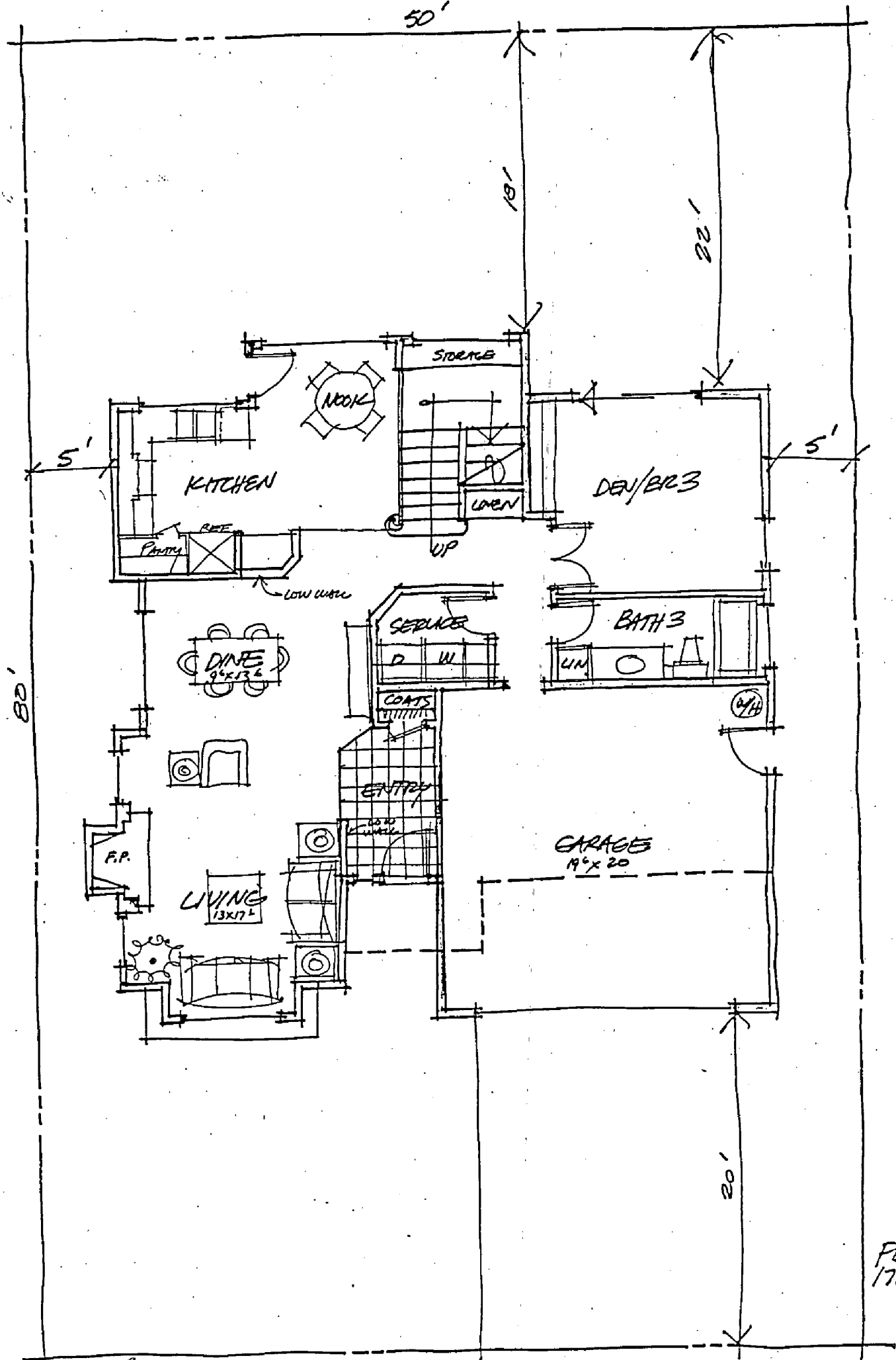
RANCH
 P.U.D.

POST
 274-030-051

RECLAMATION
 DISTRICT #1000
 274-0030-053

NORTH DISTRICT

GARDEN HIGHWAY



PLAN
1780S

197-286

723-87

128

NOTES

RECORD OWNERS:
 RECREATION BANCOR PARTNERSHIP
 7700 COLLEGE TOWN DRIVE, SUITE 101
 SACRAMENTO, CALIFORNIA 95826

SUBDIVIDER:
 CANNON DEVELOPMENT & CONSTRUCTION CO., INC.
 1909 FOLSOM BOULEVARD, SUITE 109
 SACRAMENTO, CALIFORNIA 95826

ENGINEER:
 THE SPINK CORPORATION
 POST OFFICE BOX 2511
 SACRAMENTO, CALIFORNIA 95811

EXISTING USE AND ZONE:
 PALM, R-1A, R-2B

PROPOSED USE AND ZONE:
 LOT 5, 6 LOTS, 8-14
 LOT 4, LEVEE PARKWAY
 LOT 8, REMAINDER

TYPICAL LOT SIZE:

52' x 105' (INTERIOR)
 50' x 80' (INTERIOR)
 60' x 80' (ECCENTRIC)
 AVERAGE: 57.00 AC GROSS
 107 S.F. LOTS: 13.87 AC.
 LOT 4: 1.31 AC.
 LOT 8: 0.32 AC.
 DENSITY:

8.22 DU/AC (11.64 AC NET)
 6.92 DU/AC (115.62 AC GROSS)

P.U.R.:

274-0030-008
 PROPOSED IMPROVEMENTS: CITY OF SACRAMENTO
 WATER SUPPLY: CITY OF SACRAMENTO
 SEWAGE DISPOSAL: CITY OF SACRAMENTO
 STORM DRAINAGE: CITY OF SACRAMENTO
 FIRE DISTRICT: CITY OF SACRAMENTO

SCHOOL DISTRICT: NATOMAS UNION SCHOOL DISTRICT

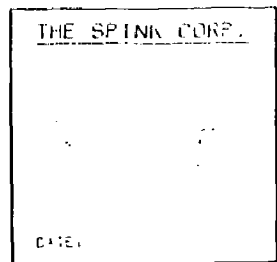
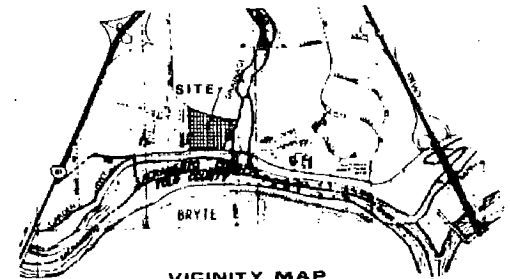
PARK DISTRICT: CITY OF SACRAMENTO
 ELECTRICITY: S.W.M.D.
 NATURAL GAS: P.G. & E.
 TELEPHONE: CITIZENS UTILITY COMPANY

LOT B .
(Remainder 0.3 AC.)

DEED LEGEND

1	8'	BLK 15 WALNUT
2	8'	ENG 15M WALNUT
3	18'	ENG 15M WALNUT
4	18'	ENG 15M WALNUT
5	18'	ENG 15M WALNUT
6	18'	ENG 15M WALNUT
7	12'	ENG 15M WALNUT
8	2-28'	ENG 15M WALNUTS
9	1-16'	ENG 15M WALNUTS
10	36'	ENG 15M WALNUT
11	102'	ENG 15M WALNUT
12	10'	BLK 15 WALNUT
13	8'	ENG 15M WALNUT
14	6' x 18'	ENG 15M WALNUT
15	20'	ENG 15M WALNUT
16	15'	ENG 15M WALNUT
17	5-8'	ENG 15M WALNUT
18	1-10'	ENG 15M WALNUT
19	1-12'	ENG 15M WALNUT
20	16'	ENG 15M WALNUT
21	20'	ENG 15M WALNUT
22	2-12'	ENG 15M WALNUT
23	6' x 14'	ENG 15M WALNUT
24	24'	ENG 15M WALNUT
25	6'	ENG 15M WALNUT
26	8'	ENG 15M WALNUT
27	14'	ENG 15M WALNUT
28	16'	ENG 15M WALNUT
29	24'	ENG 15M WALNUT
30	24'	ENG 15M WALNUT
31	14'	ENG 15M WALNUT
32	24'	BLK 15 WALNUT
33	14'	BLK 15 WALNUT
34	18'	BLK 15 WALNUT
35	10'	BLK 15 WALNUT
36	12'	BLK 15 WALNUT
37	6'	BLK 15 WALNUT
38	0'	ENG 15M WALNUT
39	8'	ENG 15M WALNUT
40	8'	ENG 15M WALNUT
41	18'	ENG 15M WALNUT
42	18'	ENG 15M WALNUT
43	4'	BLK 15 WALNUT
44	30'	BLK 15 WALNUT
45	2-20'	BLK 15 WALNUTS
46	12'	BLK 15 WALNUTS
47	6'	ENG 15M WALNUT
48	12'	ENG 15M WALNUT
49	1-18'	BLK 15 WALNUT
50	30'	BLK 15 WALNUT

NOTE: TO BE REMOVED



* LOTS 102-107 ARE STANDARD R-1 SIZED LOTS.
 * SUBDIVIDER PROPOSES TO VARY FRONT YARD SETBACKS.

TENTATIVE SUBDIVISION MAP

BRISAS DEL RIO

City of Sacramento,
 California

June, 1987



THE SPINK CORPORATION
 720 F STREET
 SACRAMENTO, CA. 95814
 (916) 444-8170

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